

# Holcomb Bridge Road East Revitalization Study



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Created by: **Robert and Company**  
96 Poplar Street  
Atlanta, GA 30303

Partnered with:

**Ken Bleakly & Associates**  
**Day Wilburn & Associates**



# HOLCOMB BRIDGE ROAD EAST

## *EXECUTIVE SUMMARY*

### Overview

During the past few years the City of Roswell has undertaken a number of planning studies and implementation projects focused on improving the quality of life for Roswell residents. The Holcomb Bridge Road East Revitalization Study is the most recent planning study conducted by the City. The purpose of this study is to identify ways to improve the connectivity of the varied uses within the corridor, an area extending from GA 400 on the west to the city's eastern edge, to enhance the vitality of the corridor's commercial development and to preserve its quality residential and environmental assets.

The study was funded by the City of Roswell and overseen by the city's Redevelopment Task Force with staff support provided by the city's Economic Development Division. A steering committee comprised of members of the Redevelopment Task Force, as well as representatives from residential subdivisions and commercial developments along the corridor, was formed at the outset of the planning process and met on a regular basis through the completion of the project.

### Existing Conditions

The major growth in the City of Roswell occurred during the 1970s, concentrated along State Route 9. With the opening of GA 400 in the early 1980s, development accelerated and spread into the study area during the 1980s and 1990s. The development patterns of the area are heavily influenced by the Holcomb Bridge Corridor as well as the environmental constraints such as the Chattahoochee River, which serves as a southern boundary of the area, and steep topography.

Within the boundaries of the study area Holcomb Bridge Road is generally a five-lane state highway linking roads to the southeast—Peachtree Industrial Boulevard, Buford Highway, I-85—with GA 400 and Alpharetta Highway to the northwest. Although a significant portion of the traffic on this corridor by-passes most of the corridor area as it heads to and from the office and retail concentrations farther to the north along Mansell and Haynes Bridge Roads and along GA 400, the roadway's high traffic volume and the presence of large residential areas nearby spurred the development of large strip-style commercial centers along the corridor. However as the suburbs of Atlanta have pushed further north, newer commercial centers such as North Point Mall, The Avenue at East Cobb and the Forum on Peachtree Parkway have drawn shoppers away from the corridor and led to the decline of older commercial centers along the corridor.

# HOLCOMB BRIDGE ROAD EAST

## EXECUTIVE SUMMARY

An inventory and analysis of existing conditions was conducted to determine the physical and market constraints affecting the redevelopment and revitalization of the corridors existing commercial centers and to identify place-making opportunities in undeveloped areas of the corridor. Emphasis was also placed on revealing existing and potential pedestrian and transportation-related issues and opportunities, destinations, and linkages among them. The existing conditions analysis includes discussion of current land use and zoning, physical features of the corridor, roadway and traffic analysis, pedestrian and cyclist connectivity and a market analysis.

### Public Involvement

Throughout the study process, which began in the late spring of 2004, the City of Roswell made significant efforts to solicit citizen input into the Holcomb Bridge Road East Revitalization Study. The intent of these efforts was to gather opinions from as wide and varied of a group of citizens as possible. Through a series of workshops, town hall style meetings and interviews, opinions regarding values, concerns and ideas about the quality of life along the Holcomb Bridge Road East corridor now and in the future were compiled. This invaluable information was woven into the Vision for the corridor and many specific recommendations have been carried through into the suggestions for the future of the area.

### Vision for the Holcomb Bridge Road East Corridor

The existing conditions analysis and public involvement process culminated in the development of a vision statement to guide the redevelopment of the Holcomb Bridge Road East Corridor. This vision is as follows:

*Holcomb Bridge Road is the east –west corridor at the center of Roswell: A community, which provides a desirable, sustainable, safe, and manageable environment for living, working, and recreation that is inclusive and supportive of the city's diverse mixture of neighborhoods, historic and cultural resources and commercial developments.*

The Redevelopment strategy developed for the Corridor is centered on three unique ideas, which were spurred by the three unique activity nodes identified along the corridor. These nodes include the areas surrounding Holcomb Bridge Road's intersections with GA 400 and Old Alabama Road, Scott Boulevard, Eaves and Road, and Nesbit Ferry Road. The strategy centers on stimulating the corridor's lagging commercial centers, connecting the areas civic and recreation institutions with its

# HOLCOMB BRIDGE ROAD EAST

## *EXECUTIVE SUMMARY*

residential neighborhoods and diversifying the community's population by incorporating a wider variety of housing options and services.

Recommended implementation measures include:

**Stimulate** the corridor by:

- Promoting and facilitating redevelopment of C-3 zoned properties such as King's Market, Market Center at Holcomb Woods (1&2), and Holcomb Bridge Crossing Shopping Center areas.
- Guiding the redevelopment of the Old Alabama Road corridor south of King's Market into a pedestrian-oriented office and mixed-use center.

**Connect** areas in the corridor by:

- Establishing a broad pedestrian and cyclist network to link neighborhoods, parks, and activity centers along the corridor via sidewalks, multi-use paths and greenways.
- Improving the aesthetic appeal and maintaining traffic flow on Holcomb Bridge Road through the use of landscaped medians and roadside tree planting.
- Furthering the development of public facilities and institutions along the corridor.

**Diversify** the corridor by:

- Using offices and higher density housing as a transitional use from retail commercial properties to single-family neighborhoods.
- Encouraging the redevelopment of existing multi-family housing to senior housing with associated medical office facilities to promote lifecycle housing opportunities.

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# **CHAPTER 1**

## *INTRODUCTION*

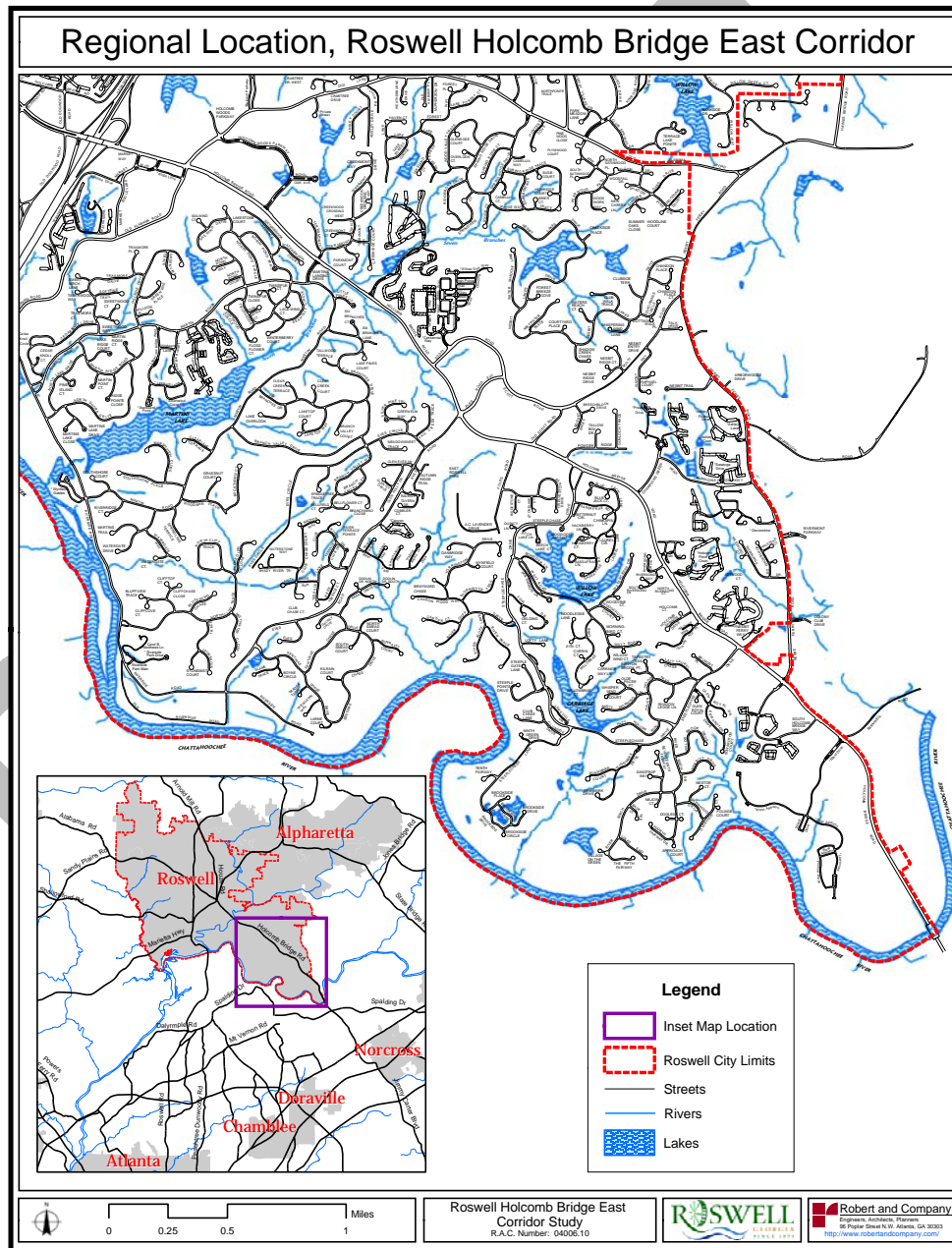
- 1.1 Study Area Location & Boundaries
- 1.2 Background Information & Study Purpose
- 1.3 Study Process
- 1.4 Vision for Holcomb Bridge Road East

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## 1.1 Study Area Location & Boundaries

The primary study area consists of the 5.1 mile Holcomb Bridge Road commercial corridor beginning at its intersection with GA 400 and extending east to the Roswell City limits (Figure 1). The study also addresses portions of other major roads intersecting Holcomb Bridge including, Holcomb Woods Parkway, Old Alabama Road, Eves Road and Nesbit Ferry Road.

Figure 1 - Study Area Map





## 1.2 Background Information & Purpose of Study

The Holcomb Bridge Road corridor is a major thoroughfare for east-west travel in the northern portion of Fulton County. The route serves as the main travel path between the city centers of Roswell and Norcross. In addition to being a connection between these locations, the Holcomb Bridge Road corridor is lined with a number of residential and commercial developments and to a lesser degree, office/professional employment centers. This mix of uses combined with the corridor's proximity to the Chattahoochee River presents an area that has the components of a thriving multi-faceted community.

However, the development of a new regional shopping area at North Point, one exit north on GA 400 has drawn regional shoppers away from the older strip shopping centers that line the gateway to the corridor around the intersections of Holcomb Bridge Road and GA 400 and Holcomb Bridge Road and Old Alabama Road, as well as those such as the Holcomb Center located near the end of the corridor at the intersection of Holcomb Bridge Road and Nesbit Ferry Road.

Excluding the commercial nodes at GA 400 and Nesbit Ferry Road the remainder of the corridor is predominantly residential. There are a number of larger subdivisions: Martin's Landing provides a mix of single family and apartment homes and Horseshoe Bend, is a master-planned community located at Steeple Chase Road. The recent Walton Centennial development at Scott Road combines a mix of apartments, town homes and single-family homes. There are also a number of apartment communities and some single family attached housing communities along the corridor. In addition to these residential developments there are many schools and civic uses located along this corridor, including the educational node at Scott Road created by the co-location of Centennial High School and Hillside Elementary.

Despite the variety of housing types and commercial shopping opportunities in the corridor there is little connectivity between the neighborhoods and retail outlets. Although most shopping centers and other commercial and office developments have sidewalks along their roadway frontage, these centers are not well integrated with nearby residential, employment, educational or civic uses. The residential developments along the corridor also lack roadway connectivity with other residential areas and the commercial centers. Most of the residential developments provide limited access points to Holcomb Bridge Road, which contributes to traffic congestion at intersections along the corridor.

The purpose of the Holcomb Bridge Road East Revitalization Study is to identify ways to improve the connectivity of the varied uses within the corridor, to enhance the vitality of the corridor's commercial development and to preserve its quality residential areas and environmental assets.

To achieve these goals this study presents a series of options for revitalizing the declining commercial developments at the eastern and western gateways of the corridor. The high concentrations of residential development along the corridor provides a strong retail market for the area, and efforts should be made to match shopping and entertainment opportunities to the varying demographics of corridor residents. Additionally, the older commercial centers provide opportunities for the development of multi-use nodes combining varying degrees of residential, commercial, and office within the same development in order to create fiscally feasible redevelopment projects that are cohesive with the surrounding residential communities.

The current lack of connectivity between the retail, office, and civic uses and the residential areas of the corridor has contributed in part to the decline of the retail environment and is a key factor in the traffic congestion experienced throughout the corridor. The study outlines methods of enhancing the existing, fragmented framework of sidewalks, bike paths and green ways in the Holcomb Bridge Road East corridor to create an integrated system of multi-modal transportation options throughout area.

### 1.3 Study Process

The Holcomb Bridge Road East Revitalization Plan was developed through a lengthy multi layered process that began with an analysis of the corridor's physical condition. Through a visual inventory of the area, the project team was able to determine the physical assets and liabilities of the study area. To compliment this physical inventory, a market overview or assessment was conducted by reviewing economic and demographic data for the Roswell/Holcomb Bridge Road area to determine the current status of the area's retail, office and residential sectors. Additionally the existing transportation system was assessed for opportunities to ease traffic congestion through intersection modifications, enhance the corridor environment through the addition of landscaped medians and streetscape elements, and provide better pedestrian and cyclist connectivity between the corridors activity centers.

At the conclusion of the assessment phase of the study the project team produced a Conceptual Master Plan for the corridor. This concept plan was based on the evaluation of the physical characteristics of the study area, comments and input received during public workshops and the determination of its suitability for a range of alternative activities and land uses. The concept plan was further refined through discussions with the Steering Committee. The purpose of this plan in the study process was to direct the market analysis phase of the study to ensure that recommended development and redevelopment scenarios for the study area are both physically and economically appropriate.

The next phase of the study included a detailed market analysis based on the land uses and development patterns reflected in the Conceptual Master Plan. The Robert and

Company Team reviewed financing alternatives to achieve the desired development objectives and prepared a series of recommended development approaches offering the best chance of achieving the corridor's potential.

Based on the recommendations of the Market Analysis a Final Master Plan was developed for the revitalization of the Holcomb Bridge Road East corridor. This master plan includes recommendations for future land use, zoning and capital improvements throughout the study area. To guide the implementation of the Master Plan an Implementation Strategy was developed to identify the roles and responsibilities of the public and private sectors, include recommendations for regulatory reforms and incentive strategies and prioritization of the projects to be undertaken to accomplish the vision of the master plan.

#### 1.4 Vision for Holcomb Bridge Road East

*Holcomb Bridge Road is the east –west corridor at the center of Roswell: A community, which provides a desirable, sustainable, safe, and manageable environment for living, working, and recreation that is inclusive and supportive of the city's diverse mixture of neighborhoods, historic and cultural resources and commercial developments.*

The Holcomb Bridge Road East Revitalization Study will have a major impact on the future of the East Roswell community and the City of Roswell as a whole.

The objective of this study is to provide guidance for the preservation and enhancement of the quality of life along this major corridor, supporting a vibrant community where individuals can live, work and play.

Stretching over five miles, the Holcomb Bridge Road East corridor encompasses a diverse grouping of land uses. A variety of revitalization strategies are required to specifically address the unique needs of each development node.

##### Stimulate

The area surrounding the intersection of Holcomb Bridge Road and Old Alabama Road near GA 400 is the gateway to East Roswell. This area provides the highest intensity of retail and office employment opportunities along the corridor. This mix and intensity of uses is appropriate and should continue, however the development of additional retail space is not encouraged. At this node the focus is on the need for greyfield redevelopment of older strip shopping centers and the reduction in the abundance of vacant retail space in the area. Redevelopment should also incorporate neo-traditional design concepts to create pedestrian oriented, mixed-use centers that include an increased amount of office space. Streetscape and other public improvements should reinforce the area as a gateway to the community. The vision for this node is a mixed-use center including pedestrian-oriented shopping environments for corporate and small business offices and retail commercial shopping opportunities.

### Connect

The Centennial node represents the mid-point of the Holcomb Bridge Road East corridor. Civic institutions and retail establishments in this area serve the adjacent large residential subdivisions. Substantial bike/pedestrian improvements along the corridor and other roadways in the area will provide better connections between uses in the immediate area and the other nodes along the corridor. Filling in the gaps in development in this node with moderate density residential infill and a mix of uses including offices and live/work town homes will assist in developing a seamless bike/pedestrian environment. The civic focus of this node also provides the opportunity for development of additional community amenities. The vision for this node is a distinctive pedestrian-oriented place that includes civic structures, neighborhood-scaled retail and commercial services, and office development that reinforce the residential nature of the surrounding area.

### Diversify

The Nesbit Ferry Node includes some of the oldest multi-family residential developments along the corridor east of GA 400. As the demographics of the community are shifting, diversifying the housing market to provide opportunities for older residents to “age in place” is a key strategy for maintaining vitality. The older multi-family developments in this node should be redeveloped into senior oriented housing including a full spectrum of independent, assisted and nursing home residential options with ancillary medical offices. These new residential developments will provide an enhanced local market for the existing retail centers in the node helping to support their renovation. Pedestrian improvements along the corridor are necessary to provide safe access from residential developments to retail centers. The vision for this node is a diverse residential environment supported by up-scale retail and office development.



Community Preference Character Sketches: Selections from among the Most Preferred Images of the Community Preference Survey.

## **CHAPTER 2**

### *EXISTING CONDITIONS*

#### **2.1 Existing Land Use**

- 2.1.1 400/Old Alabama Node
- 2.1.2 Centennial Node
- 2.1.3 Nesbit Ferry Node
- 2.1.4 Areas Between Nodes

#### **2.2 Transportation Network**

- 2.2.1 Roadway and Traffic Congestion Background Information
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- 2.2.5 Summary
- 2.2.6 Aesthetic Improvements
- 2.2.7 Pedestrian and Bicycle Facilities

#### **2.3 Demographics & Market Overview**

- 2.3.1 Demographics
- 2.3.2 Housing
- 2.3.3 Employment
- 2.3.4 Retail & Office Markets
- 2.3.5 Conclusions

An inventory and analysis of existing conditions provides a basis for the study of the Holcomb Bridge Road East corridor. This analysis process helps identify the issues and constraints within the study, specifically issues that will require decision-making by city leaders and the public. The central focus of this analysis is the identification of potential activity centers within the greater study area and the existing and potential linkages among them.

This chapter summarizes existing conditions from a variety of sources including existing city documents and plans, filed observations, aerial photography, Census statistics and real estate market information. The sections of the chapter include the following:

1. Existing Land Use and Current Zoning
2. Transportation Network
3. Corridor Demographics and Market Conditions

## 2.1 Existing Land Use

The existing land use map (Figure 2) provides a look at the current land uses in the study area: single and multi family residential, commercial, office, parks and open space, transportation/communication/utilities, and undeveloped areas. The Holcomb Bridge Road corridor, as well as the environmental constraints such as the presence of the Chattahoochee River and steep topography heavily influences the development patterns of the area.

The major growth in the City of Roswell occurred during the 1970s, concentrated along State Route 9. With the opening of GA 400 in the early 1980s, development accelerated and spread into the study area during the 1980s and 1990s.

Holcomb Bridge Road, in the study area (the Corridor), is generally a five-lane state highway (GA 140) linking roads to the southeast—Peachtree Industrial Boulevard, Buford Highway, I-85—with GA 400 and Alpharetta Highway to the northwest. Although a significant portion of the traffic on this corridor by-passes most of the corridor area as it heads to and from the office and retail concentrations farther to the north along Mansell and Haynes Bridge Roads and along GA 400, the roadway's high traffic volume and the presence of large residential areas nearby spurred the development of large strip-style commercial centers along the corridor. However as the suburbs of Atlanta have pushed further north, newer commercial centers such as the North Point Mall area, the Avenue at East Cobb and the Forum on Peachtree Parkway have drawn shoppers away from the corridor and led to the decline of older commercial centers along the corridor.

Existing land use in the study is primarily residential in nature, and mostly single family, suburban style cul-de-sac neighborhoods with corresponding zoning classifications of R-

5 Planned Residential Development and R-1. The majority of these neighborhoods were developed in the late 1980's and 1990's as master planned communities with amenities such as swim and tennis clubs. While many of the communities have sidewalks and internal bike trails they are on the whole oriented to automobile transportation as they lack connectivity with each other and their limited access points tend to isolate them from the commercial, institutional and other uses along of the corridor.

Parks, recreation and conservation areas make up a significant portion of the area accounting for just over 8%. East Roswell Park, located on Fouts Road is a 38.7-acre facility with amenities including a recreation center, ball fields and tennis courts. There are a number of institutional uses in the study area including churches and schools. Additionally, a large portion (17.4%) of the study area's land remains undeveloped.

Outside of its residential neighborhoods, institutions and parks, the corridor is characterized by a series of strip commercial and freestanding highway commercial and convenience centers. There are some office complexes located in the study area as well; the largest of these is that of Kimberly-Clark which occupies a 98-acre campus in the northeastern quadrant of Holcomb Bridge Road at GA 400.

As would be expected in a corridor study, Transportation/Communication/Utilities land uses account for a large portion of land use in the study area (10.9%) the majority of which is the Holcomb Bridge Road thoroughfare and its associated right-of-way.

Table 1: Existing Land Use - Roswell Holcomb Bridge East

Existing Land Use	Acres	%
Residential - Single-Family	3,064.5	43.8%
Residential - Multi-Family	549.0	7.8%
Commercial	235.4	3.4%
Office/Professional	136.6	2.0%
Industrial	25.8	0.4%
Public/Institutional	237.7	3.4%
Parks/Recreation/Conservation	572.5	8.2%
Forest/Undeveloped	1,218.0	17.4%
Transportation/Communication/Utilities	764.5	10.9%
WATER	199.9	2.9%
TOTAL Study Area	7,004.0	100.0%

Figure 2: Existing Land Use

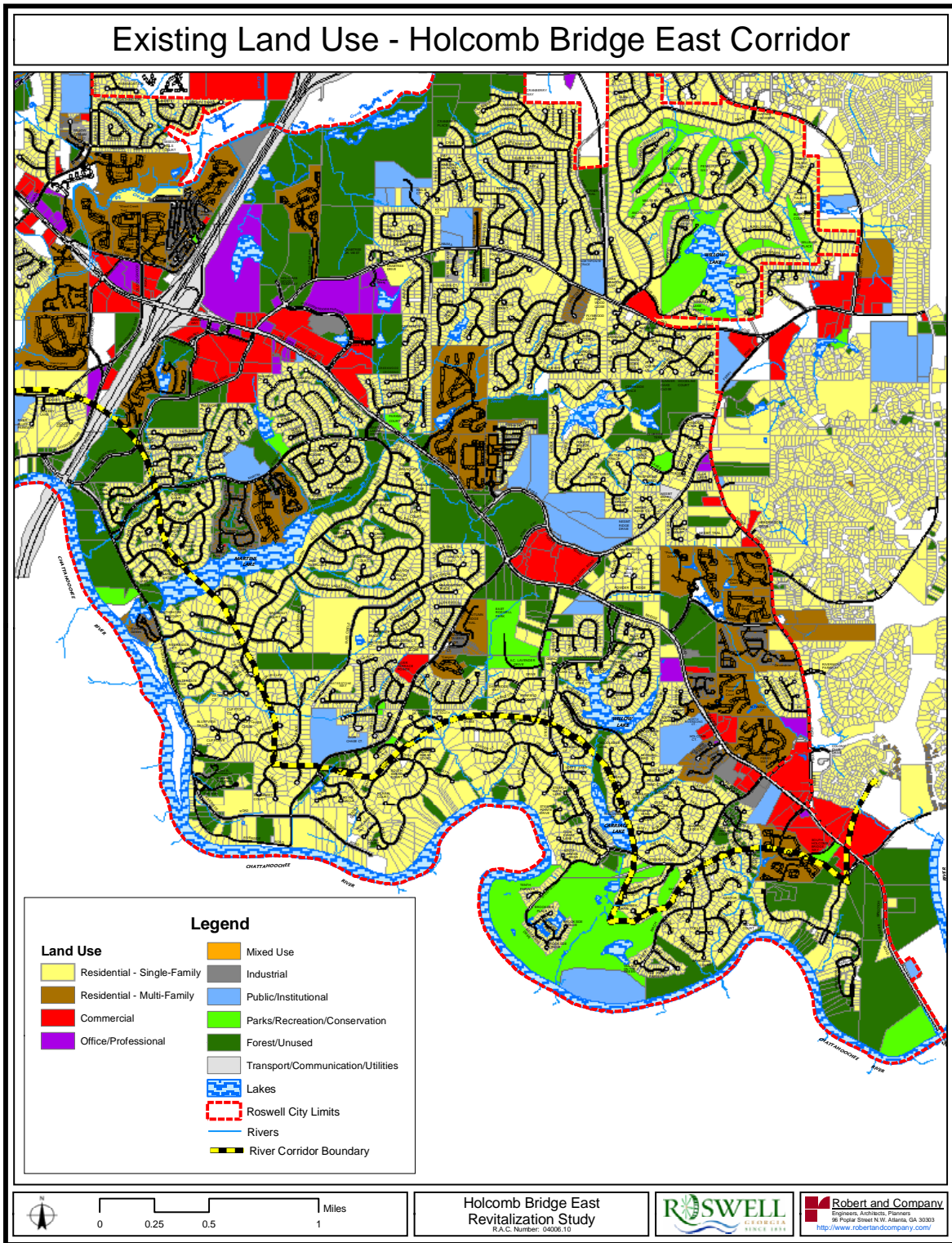
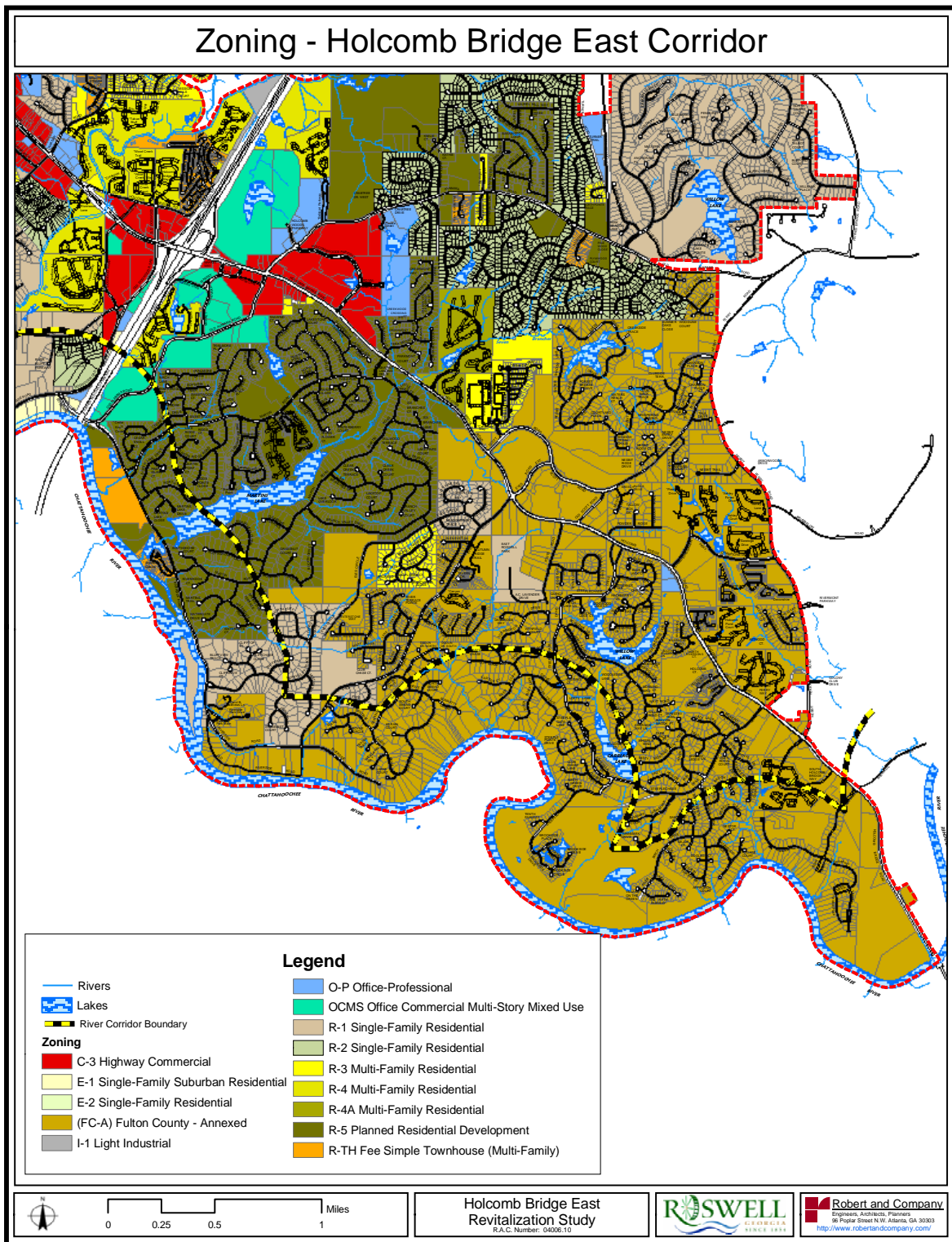


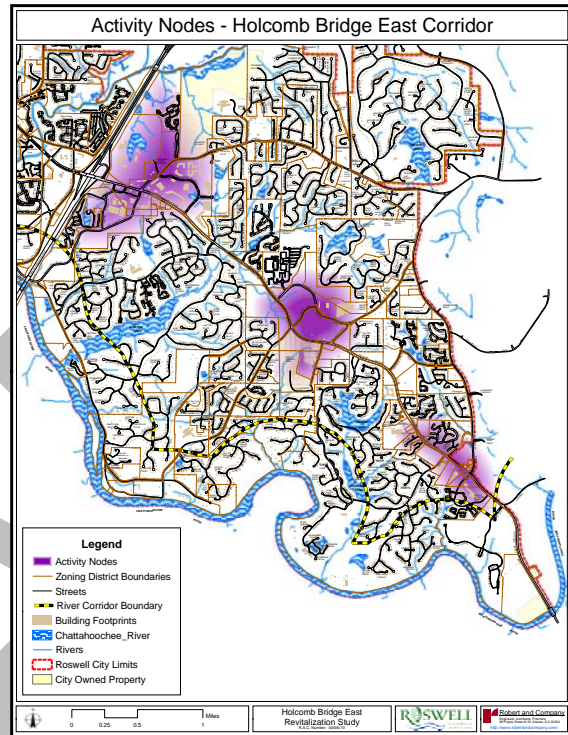


Figure 3: Current Zoning



For the Holcomb Bridge Road East corridor, there are existing conditions that support the concentration of development into nodes and restriction of development along the corridor areas that are between nodes. There are several very large master-planned residential communities that have been developed over the past several decades that have access from Holcomb Bridge Road. These communities, primarily including Martin's Landing and Horseshoe Bend, have concentrated entrance roads at a few locations and have preserved extensive areas of Holcomb Bridge Road frontage as essentially undevelopable for commercial uses by bringing the rear lot lines of residences either to or near the road right-of-way. Following analysis of these conditions and other constraints on corridor-oriented development, such as severe topographic conditions in some areas, three general nodes were identified as areas appropriated for concentrations of development along the corridor. These nodes are have been named according to their location: 400/Old Alabama Node, Centennial Node and Nesbit Ferry Node (Figure 4). The existing conditions in each of these nodes are discussed in the following sections.

Figure 4: Planning Nodes



### 2.1.1 400/Old Alabama Node

The 400/Old Alabama Node is the commercial and office activity center that extends along Holcomb Bridge Road for approximately one mile from the GA 400 interchange to (and slightly beyond) Holcomb Woods Parkway. The area identified as the 400/Old Alabama Node also extends along approximately two miles of the frontage of Georgia 400.

#### *Existing Physical and Market Conditions*

The 400/Old Alabama node is recognized as a location for corporate and professional offices as well as large retailers. Major corporations, such as Kimberly Clark, have made long-term commitments to office properties in this location, which has strong assets of accessibility to the transportation system and a highly educated workforce. While there has been some churning of retail properties, commercial property is still considered to be very valuable and the area desirable for retailers.

The two largest existing commercial centers, King's Market and Holcomb Woods Center, have experienced significant amounts of vacancy in recent years. Currently, a new

Publix supermarket is under construction at the King's Market center. While this store is expected to be very popular with the community and attract shoppers to the center, King's Market will also experience a significant tenant loss in the near future when Home Depot moves from its present location to a site on Holcomb Woods Parkway that is currently being redeveloped. Holcomb Woods Center should benefit from the new Home Depot location and other potential new tenants in the short term. While the current value of commercial property is very high in the area, the long term prospects of both of these centers are uncertain due to their aging infrastructure and the general design of the centers which has become outdated in comparison with contemporary high-end retail environments.

Secondary to retail commercial in the 400/Old Alabama Node is professional office use. Based on market surveys, there are approximately 600,000 square feet of office space in the area currently, not including the Kimberly Clark corporate campus. Existing office developments range from smaller office-condo complexes to the 67.3 acres at Kimberly Clark. Due to location advantages such as immediate access to SR400, there is great potential for additional future office development. Existing zoning (OCMS) will support a significant amount of office development along Old Alabama Road south of Holcomb Bridge Road.

Though it would be an asset to the City to see additional office development in the future, it can also be anticipated that demands will be placed on infrastructure, roads and the environment. Sustainability should remain a primary concern and goal in association with future office development, and guidance should be provided to support mixed-use, pedestrian-oriented environments that are more sustainable (fiscally and environmentally) than are many recently developed auto-oriented office environments (note Perimeter Center which is expending significant funds to combat congestion).

Figure 5 Existing Land Use in Old Alabama / GA 400 Node

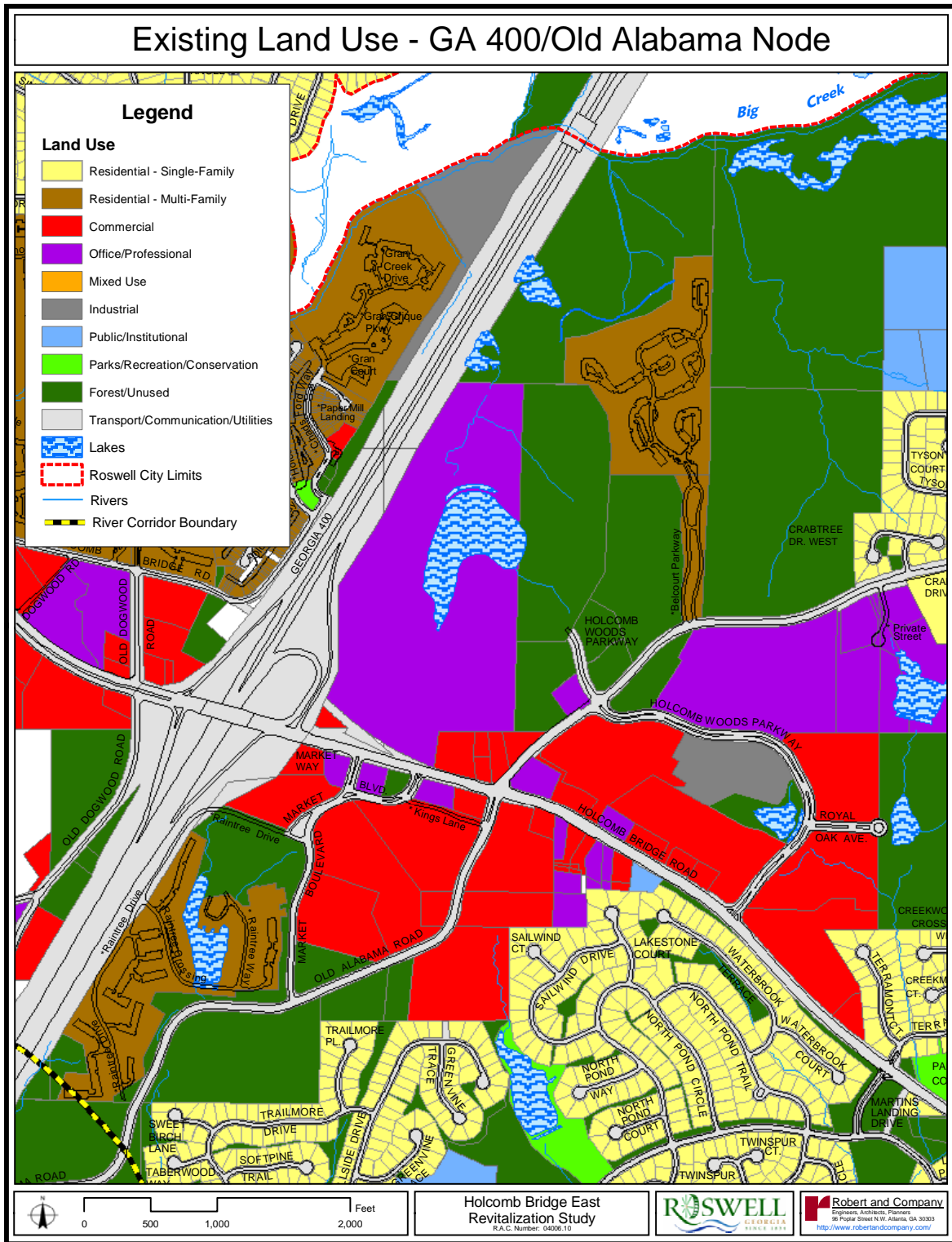
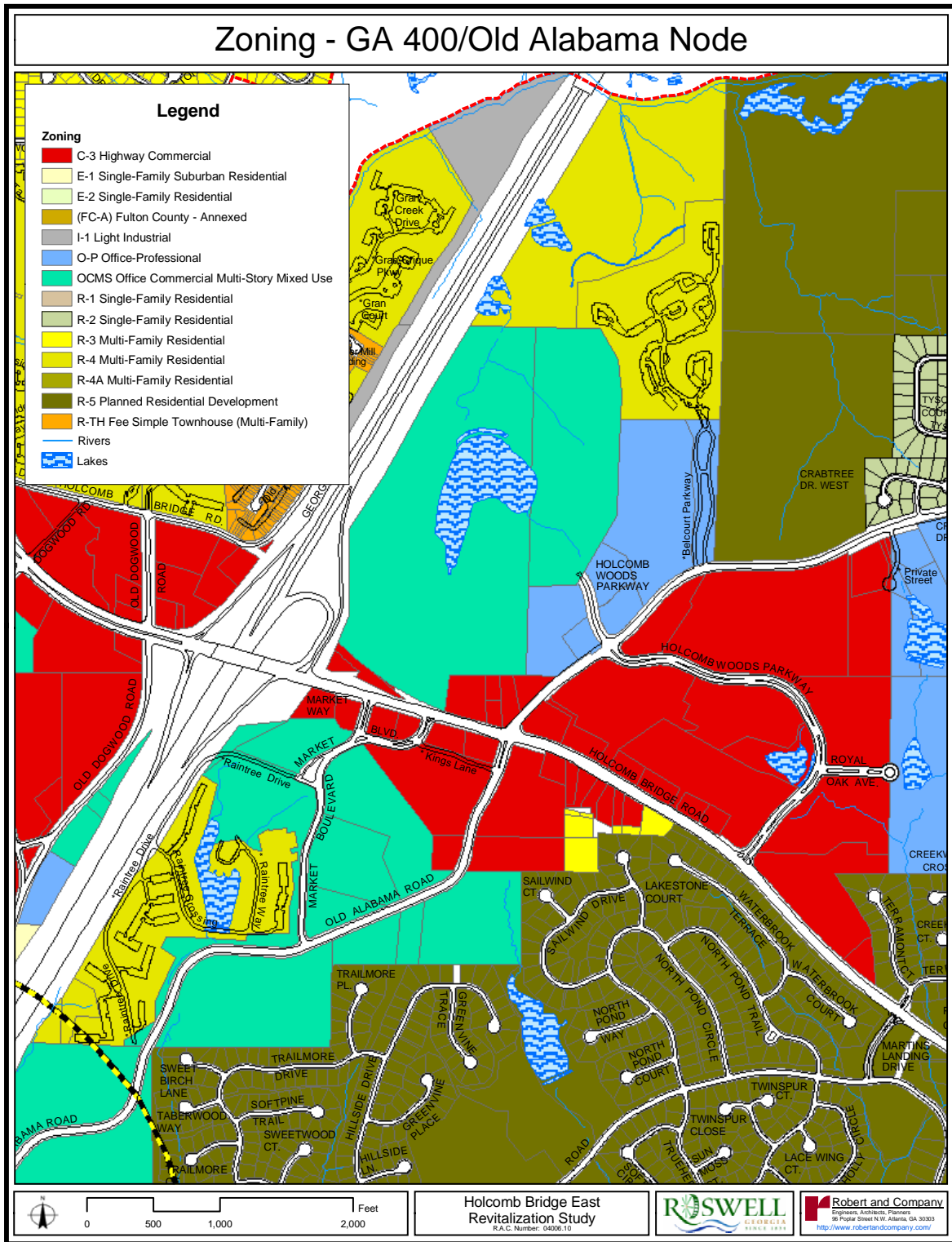


Figure 6 Existing Zoning in Old Alabama / GA 400 Node



### 2.1.2 Centennial Node

The Centennial Node is the immediate vicinity surrounding the intersections of Eves Road with Holcomb Bridge Road and Scott Road. Existing land uses in the node include major community institutions, a neighborhood shopping center and medium to low-density residential properties. The Centennial Node is surrounded by residential neighborhoods of varying densities. There is also a significant amount of undeveloped land within the Centennial Node.

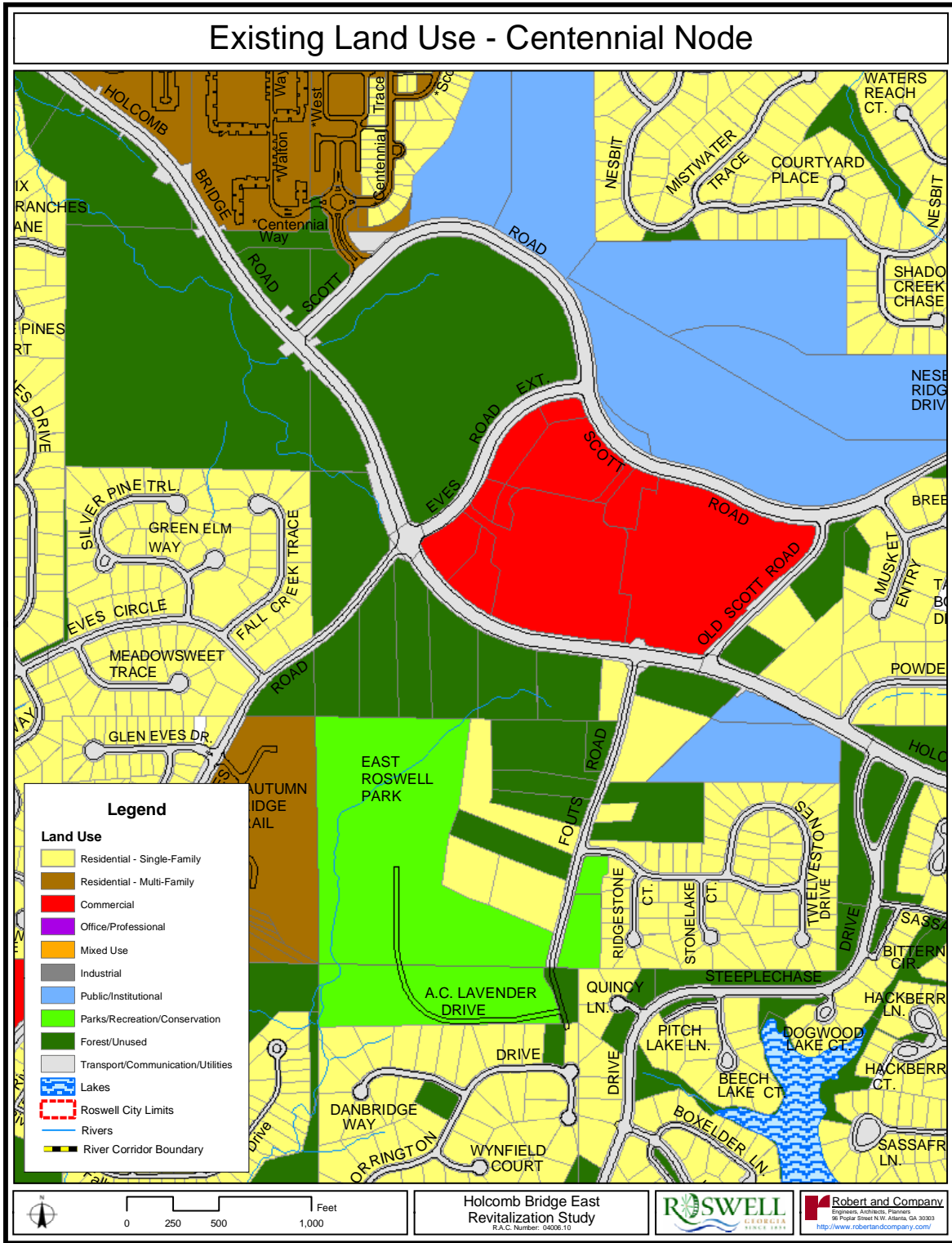
#### *Existing Physical and Market Conditions*

The Centennial node is home to major local institutions including Centennial High School, Hillside Elementary School and East Roswell Park. For this reason, the Centennial area is considered by many to be the community center of the East Roswell area. The Centennial Village Shopping Center, located between Centennial High School and Holcomb Bridge Road, is the most recently developed commercial center along Holcomb Bridge Road and is mostly occupied with major tenants including large Kroger and Kohl's stores. Though located in this community center environment, the design of the Centennial Village Shopping Center is consistent with an auto-oriented, "highway commercial" development model.

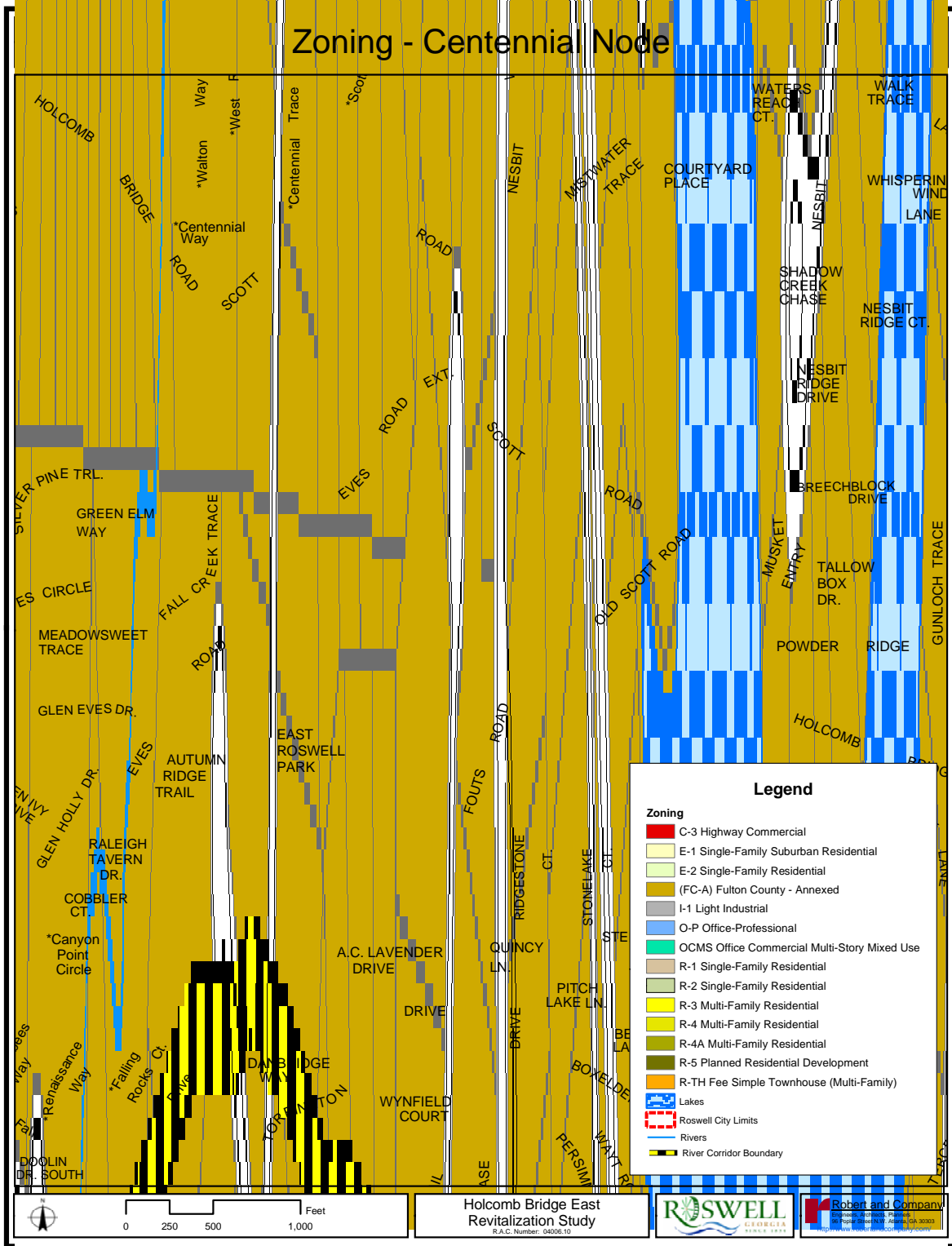
Recent residential development in the Centennial node has been very successful in terms of sales and value. The Walton Centennial development of mixed density housing, including single-family detached, townhomes, and apartments, has performed very well in the market. Townhomes, in particular, currently have a very high value relative to the overall housing in the area. Design of Walton Centennial was based on some traditional town planning concepts, such as interconnected streets and blocks, however the community is self-contained and there are no pedestrian connections to commercial or office uses.

Vacant/undeveloped land in the center of the Centennial node presents an important opportunity for both development and open space preservation. Two tracts in particular are located with frontage on either side of Holcomb Bridge Road to the west of the Eves Road intersection. On the north side, a 28-acre tract lies between Holcomb Bridge and Centennial High School, bordered by Scott Road. Zoned for office development, proposals for this developable tract have been discussed for several years. On the south side of Holcomb Bridge, approximately 49 acres of undeveloped land extend from Eves Road west almost reaching the entrance to Martins Landing subdivision. This terrain includes very steep slopes and a creek that runs parallel to the roadway. Due to the high values of residential and commercial property in the area, it is assumed that these areas of undeveloped land have great development potential from an economic standpoint.

Figure 7: Existing Land Use in the Centennial Node



Figure





### 2.1.3 Nesbit Ferry Node

The Nesbit Ferry Node surrounds the intersections of Holcomb Bridge Road with Nesbit Ferry Road, Steeplechase Drive and Holcomb Springs Drive. Existing land uses in the node include apartment complexes, retail centers and some office/institutional uses. The Nesbit Ferry Node is surrounded by residential neighborhoods and includes areas of unincorporated Fulton County as well as City of Roswell.

#### *Existing Physical and Market Conditions*

Located near the edge of the City of Roswell at Fulton County, the Nesbit Ferry Road – Holcomb Bridge Road area is significantly distanced from Roswell's city center and even GA 400. In fact, this area is almost equal distance from the centers of Roswell and Norcross. All areas under City of Roswell jurisdiction in the Nesbit Ferry area were annexed into the City in recent years.

The primary existing land uses within the Nesbit Ferry area are retail commercial (75 acres) and multi-family residential (62 acres). Of the three major retail commercial centers, two are located in unincorporated Fulton County and the third, Rivermont Square, is in Roswell. The owner of the Rivermont Square has recently invested in physical improvements to the property to enhance its attractiveness to tenants and customers, anticipating the center to be viable for the foreseeable future.

The garden apartment complexes in the Nesbit Ferry area, Tree Ridge Apartments in particular, are among the oldest and largest on the Holcomb Bridge Road East corridor. Though vacancy is relatively low and rental rates are competitive, the age of these complexes is a factor due to the tendency for older rental housing to become obsolete as lifestyles and expectations change.

The Nesbit Ferry activity center was analyzed as a possible location for senior housing and medical uses. As the Holcomb Bridge Road East corridor area includes a large number of family households, it can be expected that the heads of these households will increasingly have responsibility for aging parents. Currently, there are no significant senior housing (independent living or continuum of care) options in the area, and there are limited medical facilities in the immediate area as well.

Figure 9: Existing Land Use in Nesbit Ferry Node

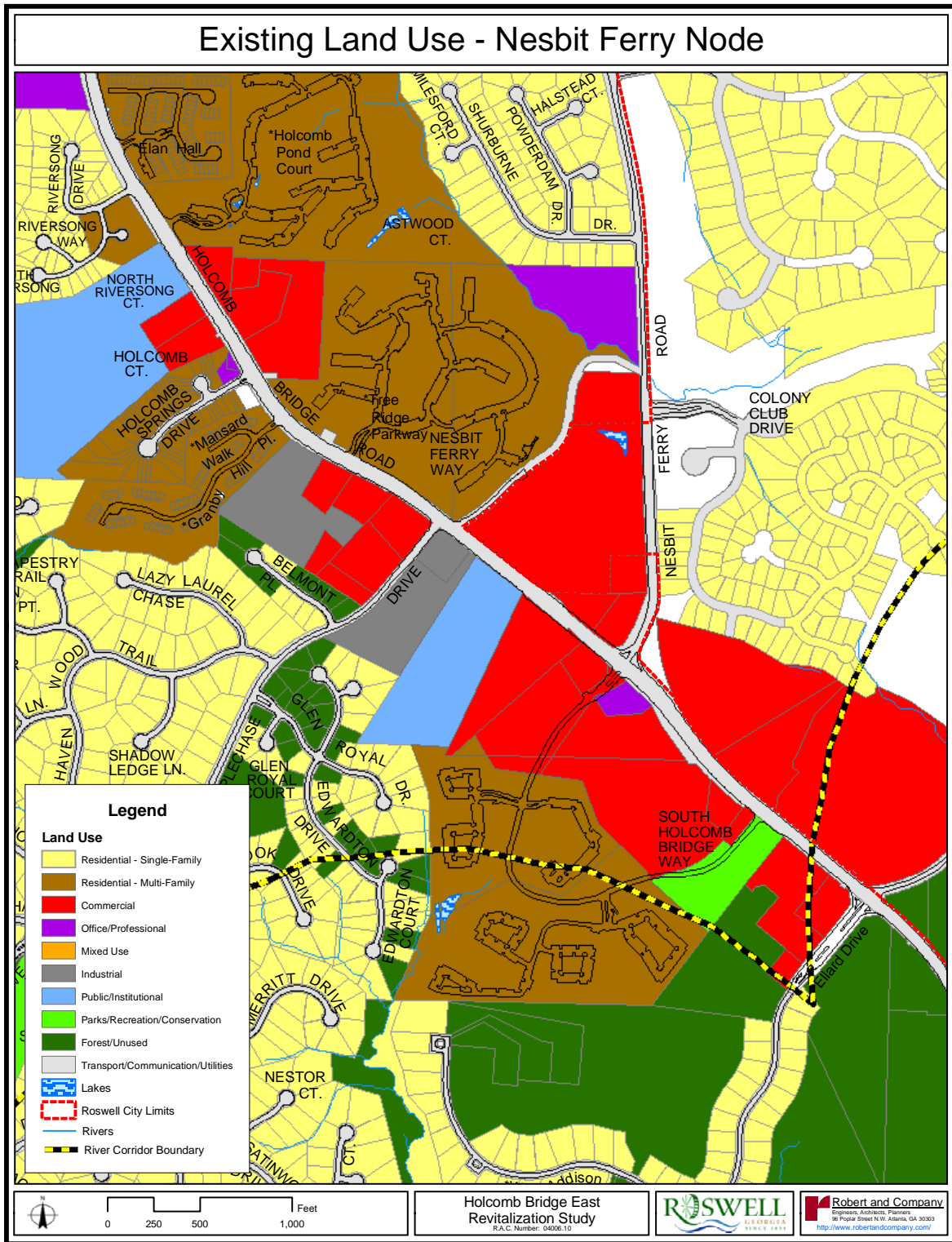
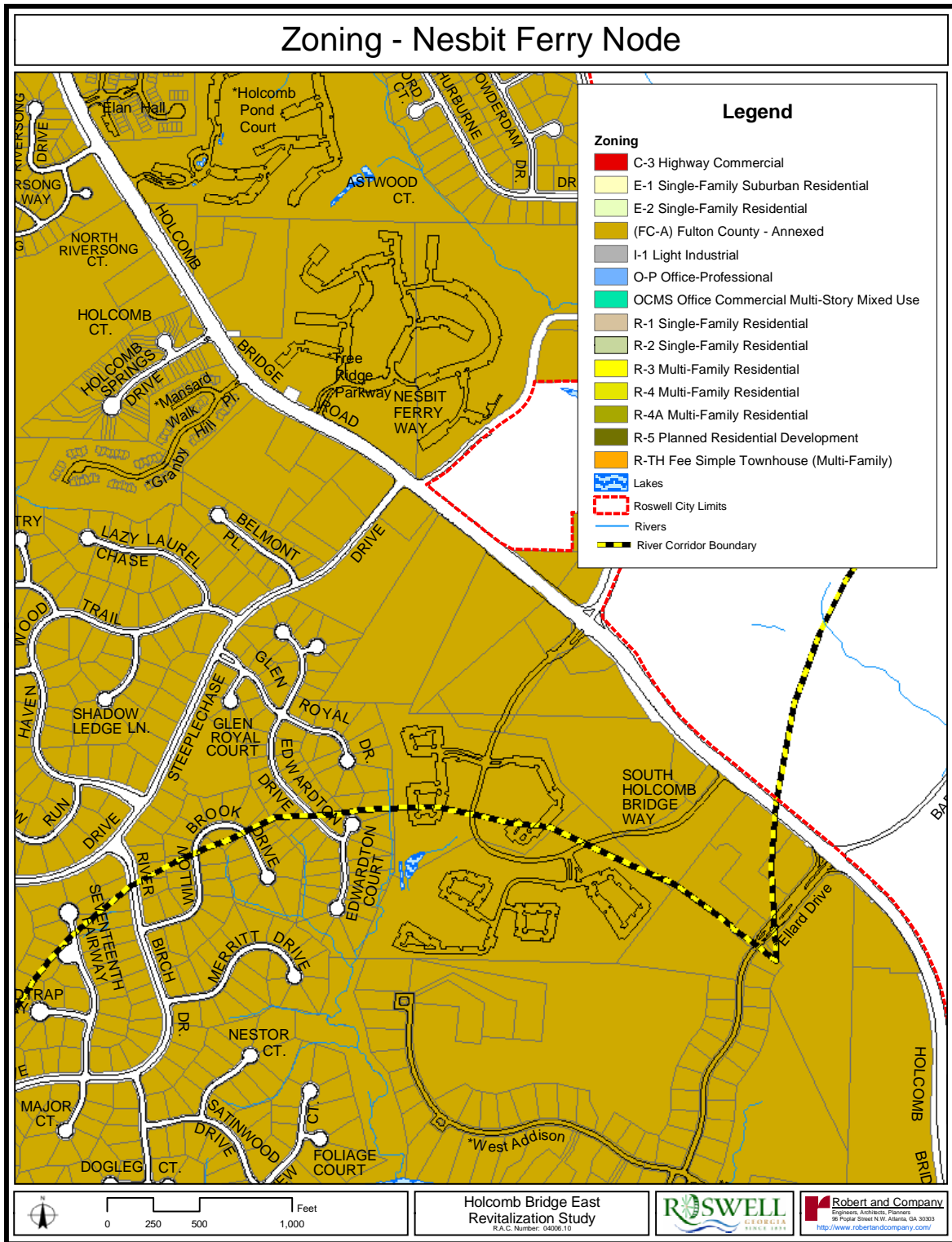


Figure 10: Existing Zoning in Nesbit Ferry Node



#### 2.1.4 Areas Between Nodes

Along the 5.1 mile length of the Holcomb Bridge Road East study area, there are approximately 2.5 miles of corridor that are located between the identified activity nodes. While they do not typically include intense development or the potential for it, these inter-nodal areas are also important from the planning standpoint.

##### *Existing Conditions*

The most significant factor that impacts the nature of the Holcomb Bridge Road corridor between the identified activity nodes is the way in which the large master planned communities were developed. Martin's Landing, Horseshoe Bend and similar neighborhood and apartment communities have entrance roads from Holcomb Bridge Road, but these entrances are consolidated at only a few locations. Between community entrances are great lengths of the corridor with no driveways or road-fronting development. This existing condition is due to the design of the communities, which has placed the rear lots of homes, usually including fences, adjacent to the road right-of-way. As a result of this, it can be expected that these lengths of Holcomb Bridge Road will remain without frontage development or curb cuts for the foreseeable future.

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## 2.2 Transportation Network

During the initial land use analysis and public participation phases of the study process it became apparent that the area of greatest concern to the Roswell community from a roadway and traffic alleviation standpoint is the section of Holcomb Bridge Road between GA 400 and Old Alabama Road. Therefore, this area has been specifically called out for roadway design and traffic analysis. Stakeholder interviews and public comments identified the greatest desire throughout the remainder of the corridor is for increased pedestrian and bicycle connectivity between the identified activity nodes and aesthetic enhancements to the roadway environment. The following sections provide an analysis of the existing traffic conditions at the GA 400 / Old Alabama node and the environment for pedestrian and cyclists along the corridor.

### 2.2.1 Roadway and Traffic Congestion Background Information

The area along Holcomb Bridge Road from GA 400 through Old Alabama Road experiences significant congestion for multiple hours during the AM and PM peak periods (7:00 – 9:00 AM and 4:00 – 6:00 PM, respectively). In conjunction with efforts to revitalize areas along Holcomb Bridge Road east of GA 400, Day Wilburn and Associates (DWA) examined the Holcomb Bridge Road corridor to determine the need for additional roadway and intersection improvements to reduce congestion.

As a major arterial and key connection to GA 400, the Holcomb Bridge Road corridor has experienced heavy traffic volumes and congestion for several years. In response to congestion problems, the City of Roswell contracted with DWA in 1999 to examine traffic operations and safety along the corridor. This study, entitled the Holcomb Bridge Road Traffic Operations and Safety Analysis, included intersection operations and geometric improvements for Holcomb Bridge Road from SR 9 to Eves Road. Figure 11 provides an illustration of the lane configuration along Holcomb Bridge Road from GA 400 to Old Alabama Road at the time of the original 1999 study.

In recent years, many of the study's recommendations were implemented, by the Georgia Department of Transportation (GDOT) in the congested area between GA 400 and Old Alabama Road. These modifications, described below, are depicted in Figure 12.

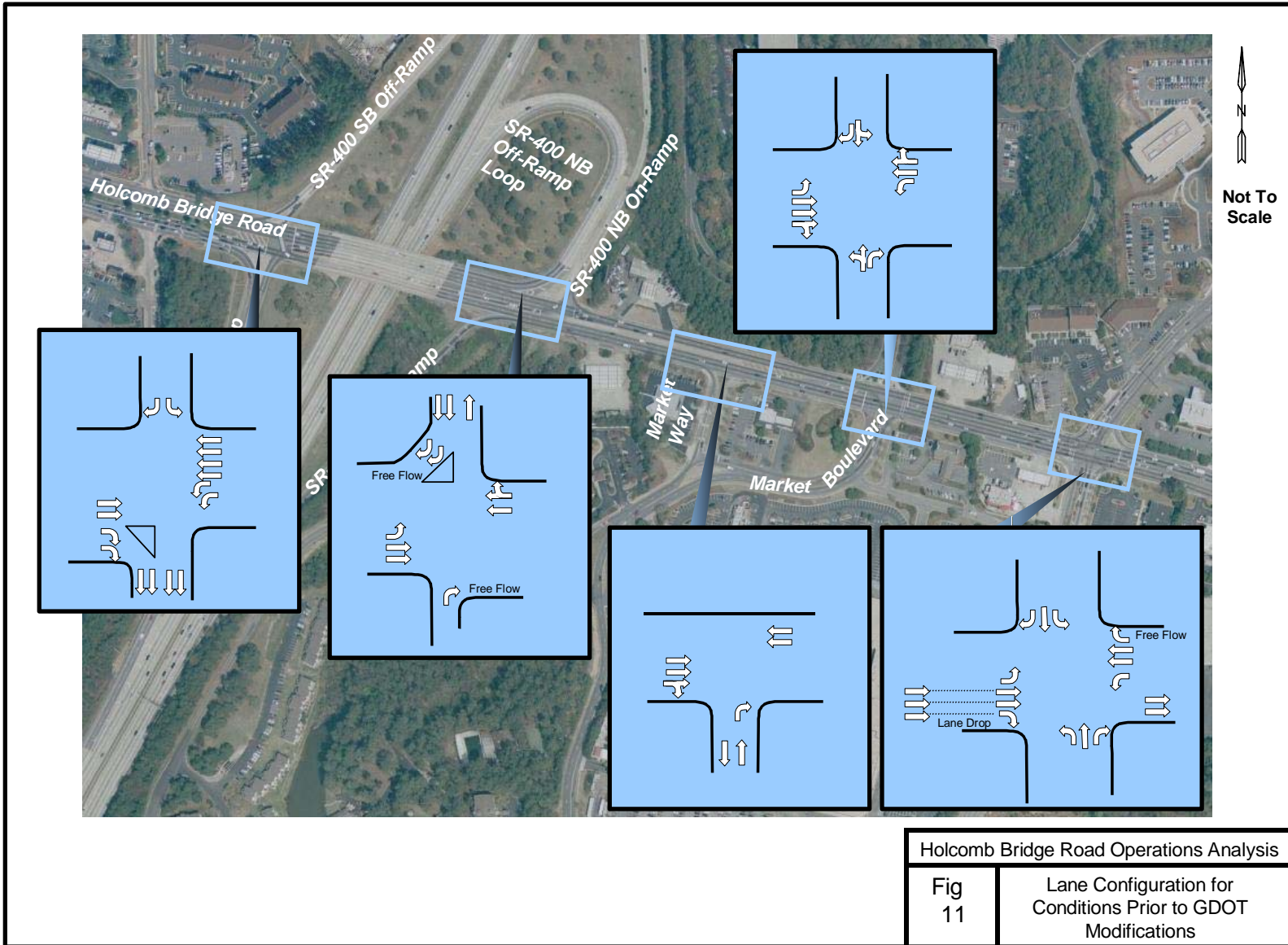
#### *GA 400 Interchange Modifications*

The GA 400 interchange was modified to increase the left-turn storage for eastbound traffic turning left onto GA 400 northbound. These vehicles previously spilled over the short left-turn lane, blocking one of the eastbound through lanes. The lane widths on the GA 400 bridge were reduced, and the bridge was re-stripped to provide this additional turning lane for the length of the bridge. In addition, a westbound right-turn lane was added for travel onto the GA 400 northbound ramp. One intersection modification was identified in the previous study for the GA 400 southbound intersection. This improvement involves modification of the southbound off-ramp to

provide dual left-turn lanes. GDOT currently has projects planned for this intersection, which include this improvement.

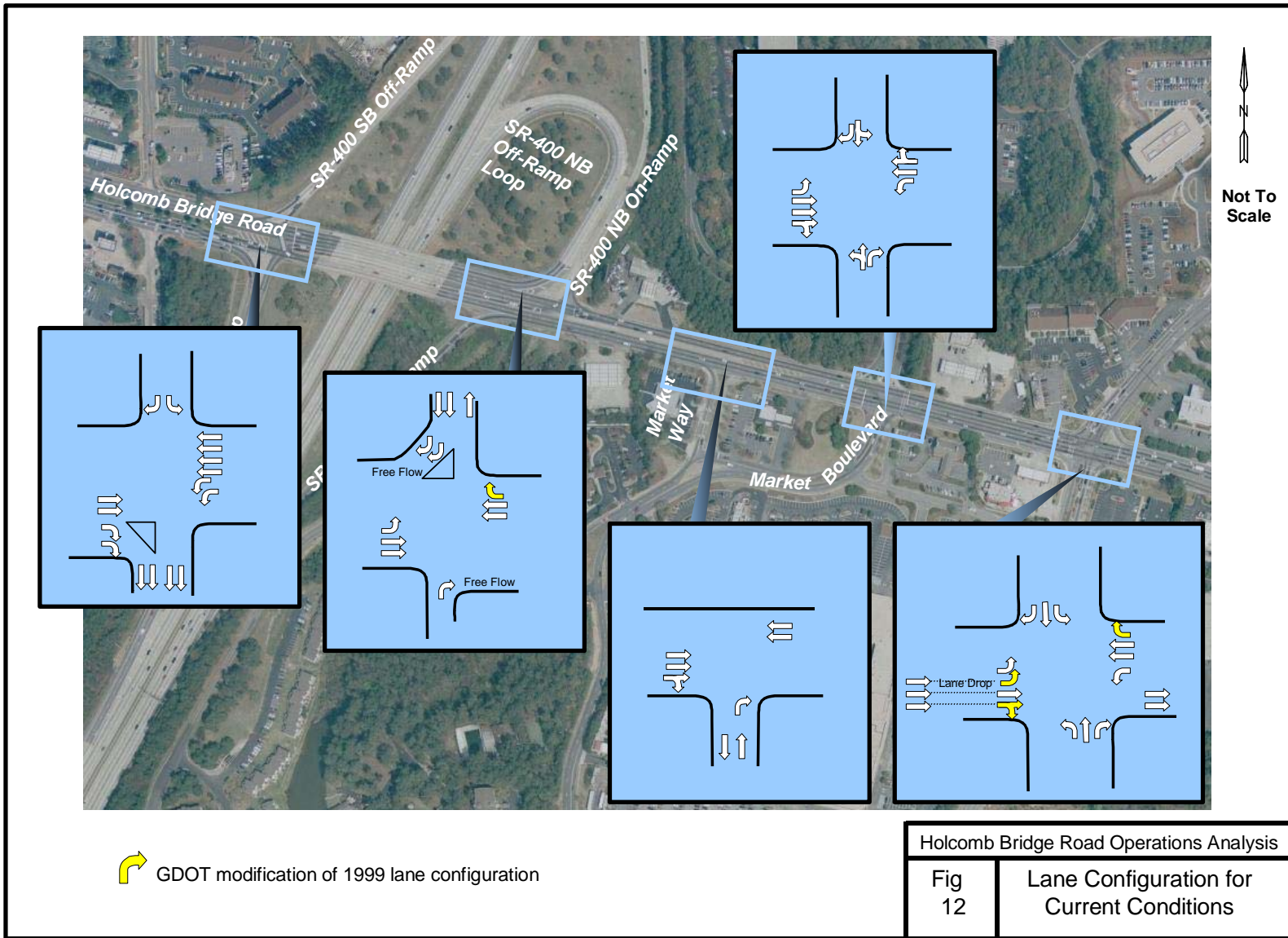
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Figure 11: 1999 Holcomb Bridge Road / Old Alabama Road Configuration



May 4, 2005

Figure 12: Lane Configurations for Current Conditions



May 4, 2005



### *Old Alabama Road Intersection Modifications*

Holcomb Bridge Road experiences heavy turning volumes to and from Old Alabama Road. These combine with heavy through movements along Holcomb Bridge Road, resulting in significant congestion at this intersection, particularly during the PM peak hour. Prior to recent improvements, the eastbound left-turn onto Old Alabama Road accommodated over 500 vehicles per hour (vph) in a single left-turn lane. This movement frequently spilled over the left-turn lane, blocking one of the eastbound through lanes.

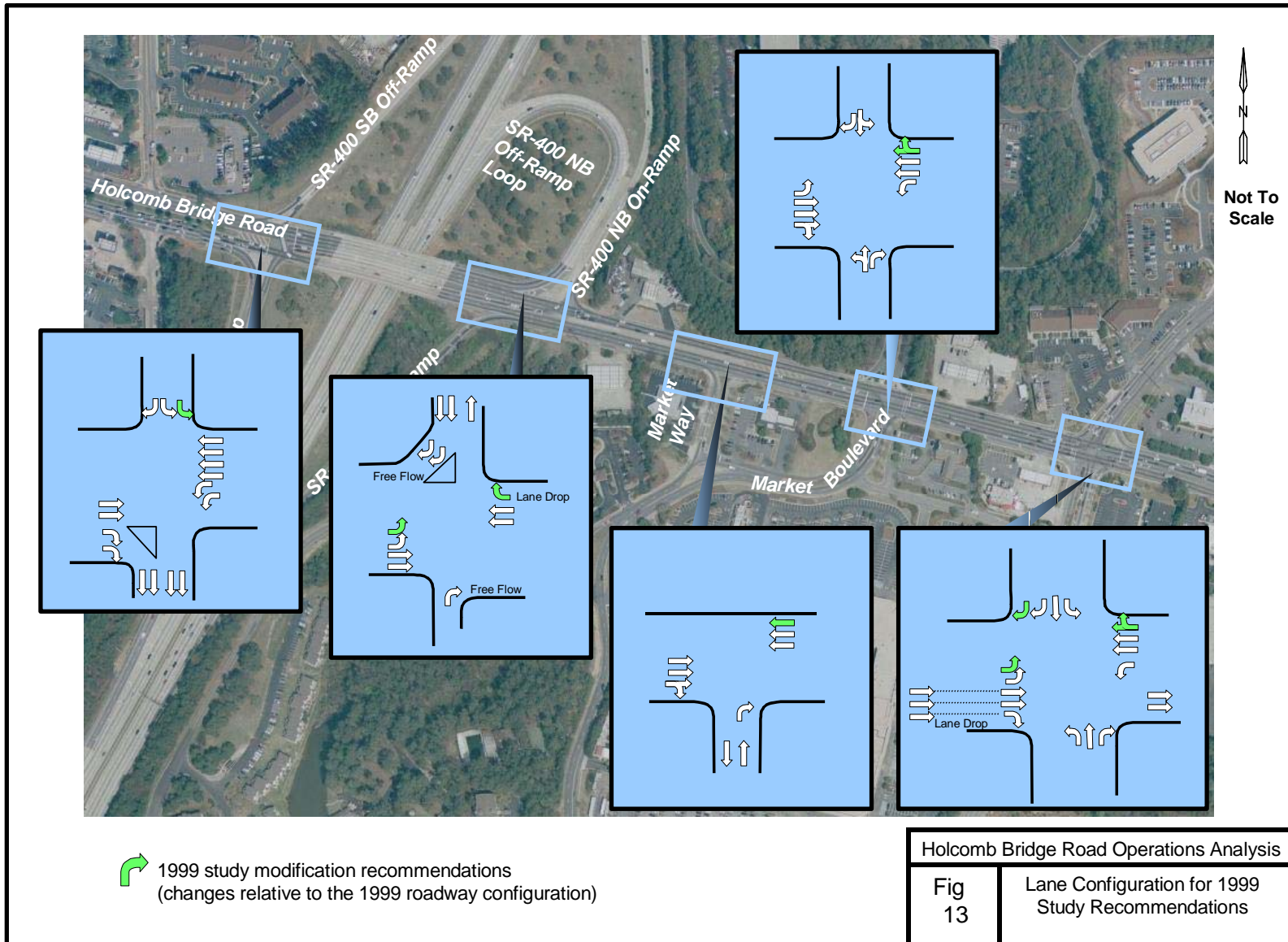
The Holcomb Bridge Road Traffic Operations and Safety Analysis included improvement recommendations for providing:

- A dual eastbound left turning movement onto Old Alabama Road,
- A dual right turning movement from Old Alabama Road, and
- An additional westbound through lane extending from east of Old Alabama Road to the GA 400 interchange.

Figure 13 provides an illustration of the resulting lane configuration of these improvement recommendations. These improvements would require additional right-of-way along the north side of Holcomb Bridge Road and the west side of Old Alabama Road. Since the acquisition of right-of-way was a concern, the study included two alternative sets of improvements to reduce the impacts of construction. One alternate included a reduced amount of widening without an additional westbound lane. Another alternate retained the existing curb line along the north side of Holcomb Bridge Road without the additional westbound lane and utilized a drop lane for Holcomb Bridge Road eastbound into the dual left-turn lanes at Old Alabama Road.

In a recent GDOT improvement project, the second of these alternatives was implemented. Though this alternative does not impact the curb line, it includes a lane drop into the eastbound dual left-turn lanes. This lane drop creates some confusion for drivers traveling eastbound on Holcomb Bridge Road. The City of Roswell installed special signage to provide advance warning of the lane shift near the Market Boulevard intersection and east of the GA 400 interchange to provide guidance to motorists traveling eastbound.

Figure 13: Lane Configurations for 1999 Study Recommendations



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Figure 14 shows the existing signage used to draw driver’s attention to the eastbound lane drop at Old Alabama Road. Just east of the Market Boulevard intersection, a left-turn arrow hangs over the northernmost through lane (via span wire) with lettering reading “This Lane Old Alabama Road (left-turn arrow) 830 feet.” The similarity of this sign to the standard left-turn only sign mounted nearby may lead drivers to believe that the lane is utilized for a dual left-turn into the Kimberly Clark industrial property.

**2.2.2 Existing Traffic Operations**

In order to assess the traffic operations with the new roadway configuration, turning movement counts were conducted along Holcomb Bridge Road. Figure 15 provides a summary of the turning movement counts along the corridor during the AM and PM peak hours.

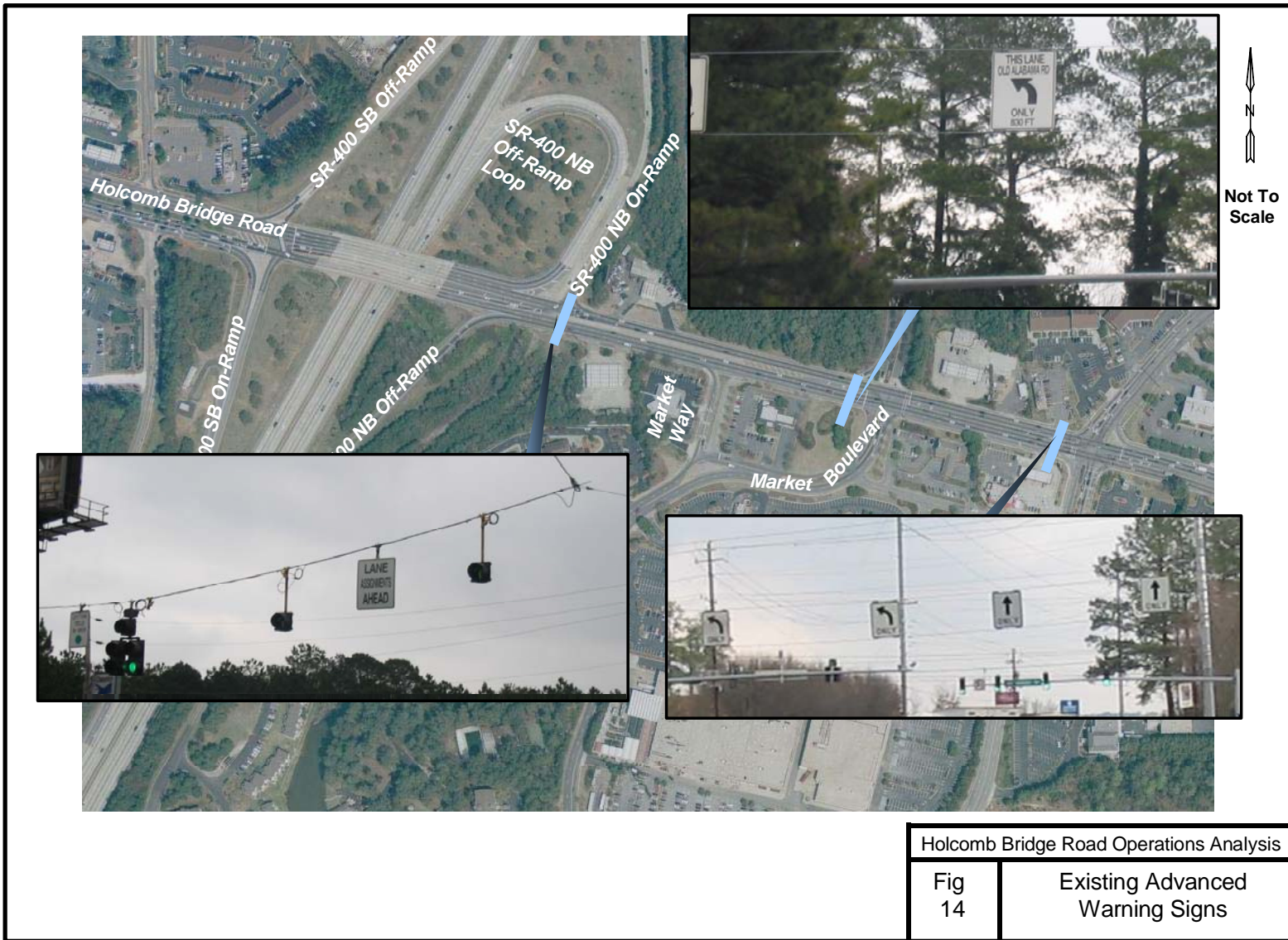
Using the 2005 turning movement counts, capacity analyses were performed during the AM and PM peak periods for the signalized intersections of Holcomb Bridge Road at GA 400 northbound, GA 400 southbound, Market Boulevard, and Old Alabama Road. The capacity analysis was quantified according to level of service (LOS) and intersection delay. LOS ranges from “A” to “F” based on the average delay in seconds experienced per vehicle entering the intersection. In general, LOS E, which correlates into an average delay of greater than 55 seconds per vehicle at the intersection, is considered unacceptable by most motorists, typically resulting in congestion and failure to clear the intersection within one cycle of the adjacent traffic signal. Table 2 summarizes the results of the existing conditions analysis.

**Table 2  
Existing Delay and Level of Service along Holcomb Bridge Road**

Intersection	AM Peak Hour		PM Peak Hour	
	Delay	LOS	Delay	LOS
Holcomb Bridge Road at SR 400 SB	30	C	90	F
Holcomb Bridge Road at SR 400 NB	8	A	45	D
Holcomb Bridge Road at Market Boulevard	29	C	23	C
Holcomb Bridge Road at Old Alabama Road	48	D	141	F

As this table shows, the PM peak hour is the most congested time period along Holcomb Bridge Road, with the GA 400 SB and Old Alabama Road intersections operating at LOS F. Field observations and traffic volume data indicate the PM peak hour conditions occur for multiple hours during the PM peak period (4:00 PM through 6:30 PM).

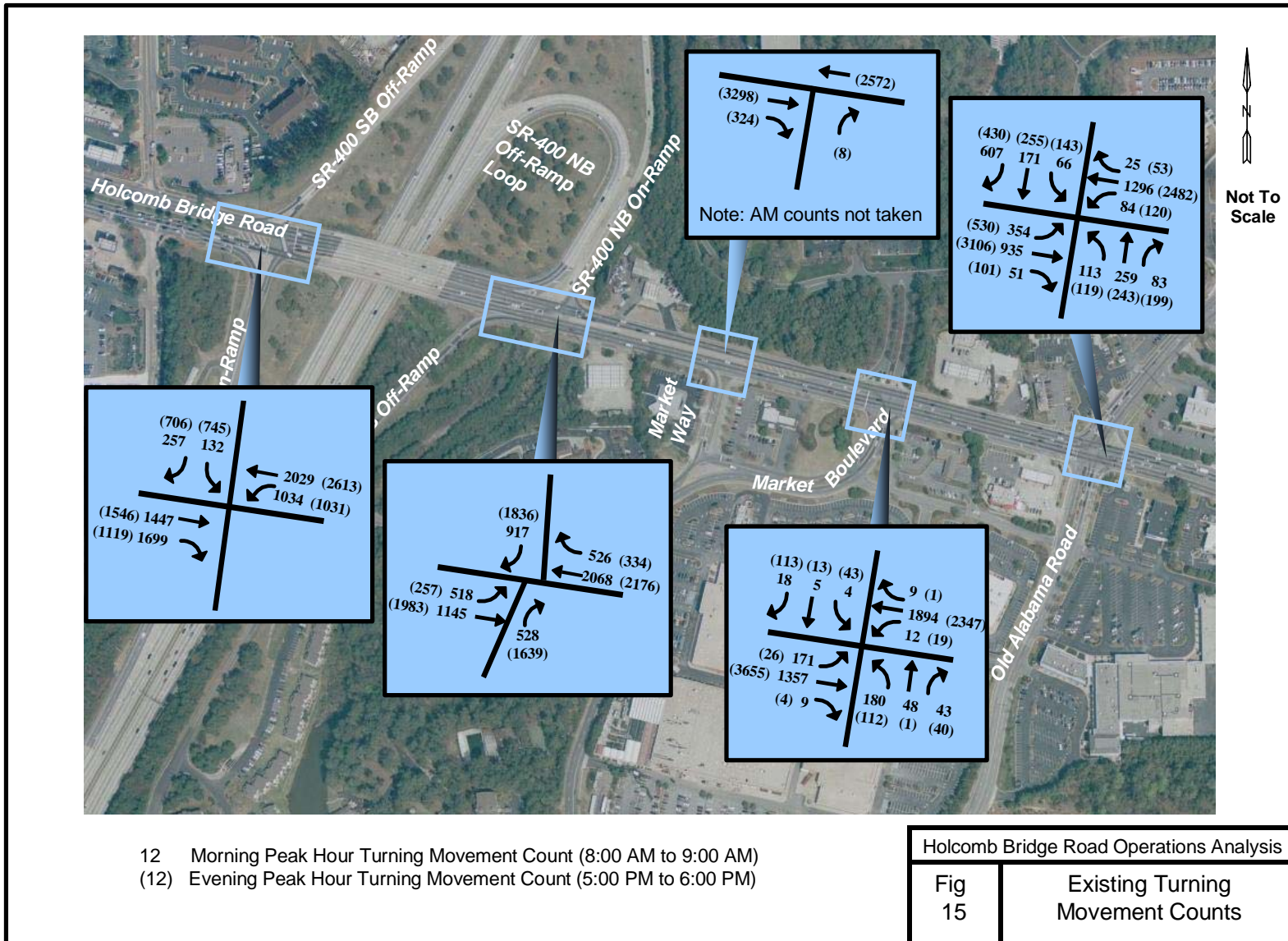
Figure 14: Existing Advanced Warning Signs



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Figure 15: Existing Turning Movement Counts



May 4, 2005



### 2.2.3 Traffic Growth

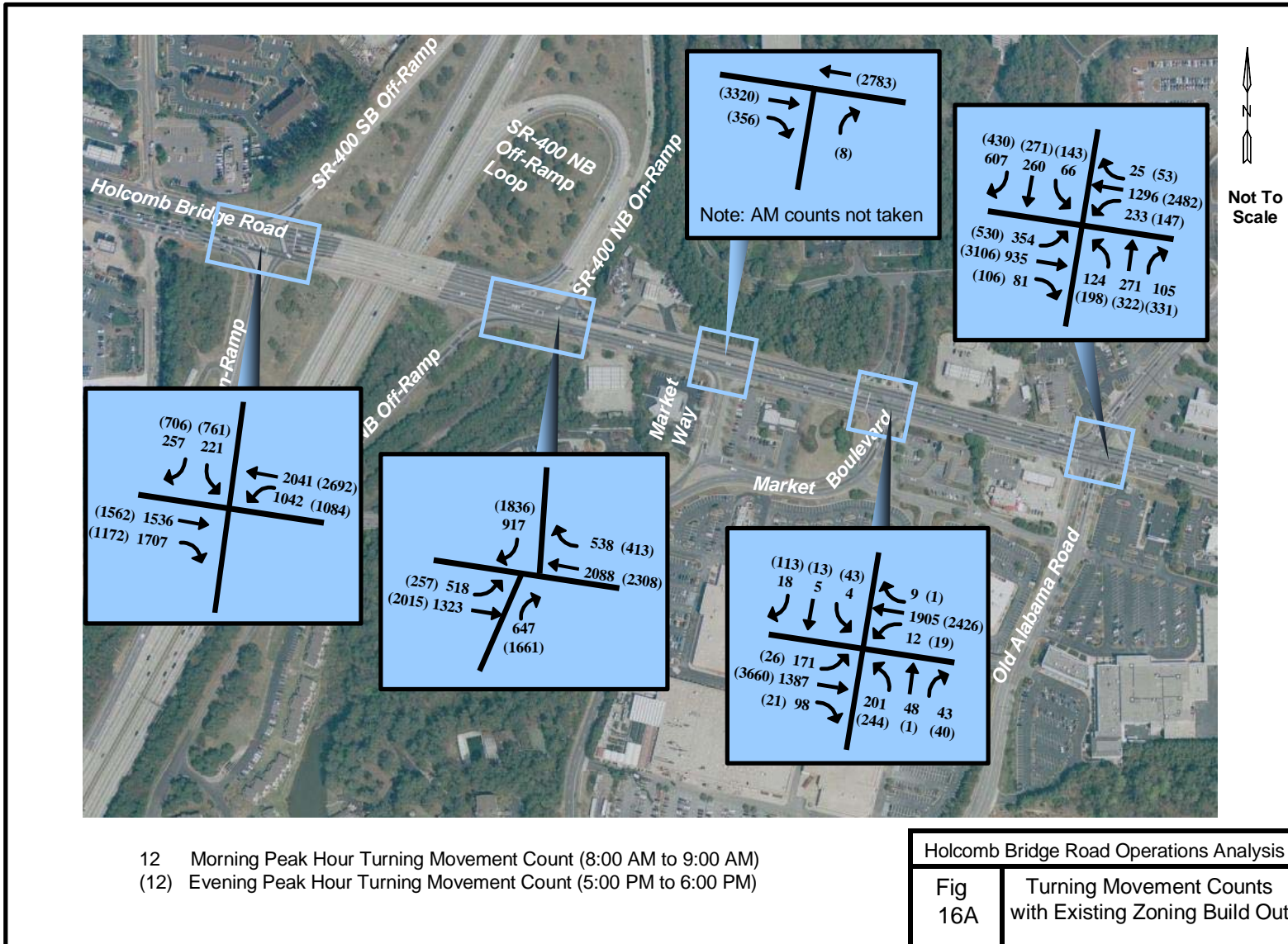
An examination of GDOT count station data from 1997 through 2003 indicates the Holcomb Bridge Road corridor has experienced no significant growth in overall daily traffic volumes (Table 3). This may be due to the fact that critical corridor intersections are experiencing saturated conditions during the peak hour, causing new trips to select alternative routes.

**Table 3**  
**Traffic Growth from 1997 Through 2003 Along Holcomb Bridge Road**

Station #	Location	Growth Rate
TC 346	SR-140 east of Warsaw Road	-2.30%
TC 348	SR-140 east of Dogwood Road	-1.20%
TC 801	SR-140 west of Old Alabama Road	-0.20%
TC 803	SR-140 east of Scott Road	1.50%
TC 804	SR-140 at Chattahoochee River	-0.60%
TC 863	Old Alabama Road north of SR-140	0.20%
TC 865	Old Alabama Road west of Old Alabama Connector	-2.20%

Although the traffic volumes along the corridor have remained steady for the last several years, current zoning allowances include the addition of 425,000 square feet of office space along Old Alabama Road south of Market Boulevard. The revitalization plan for the area includes an additional 1,000,000 square feet of office space. These increases in office space are likely to increase vehicle trips along the Holcomb Bridge Road corridor. This additional traffic was projected based on standard trip generation rates for general office space provided in the Trip Generation Handbook by the Institute of Transportation Engineers (ITE). Given the poor existing intersection operations at Old Alabama Road and the presence of existing capacity on Market Boulevard, additional traffic traveling to/from the west generated by this development would use the signal at Market Boulevard for access until that intersection becomes more congested. Traffic traveling to/from the north and east is assumed to primarily use the Old Alabama Road intersection. Figure 16A provides a summary of the future traffic volumes with the extra office development due to build-out of current zoning and Figure 16B shows the future traffic volumes with additional office space from planned redevelopment.

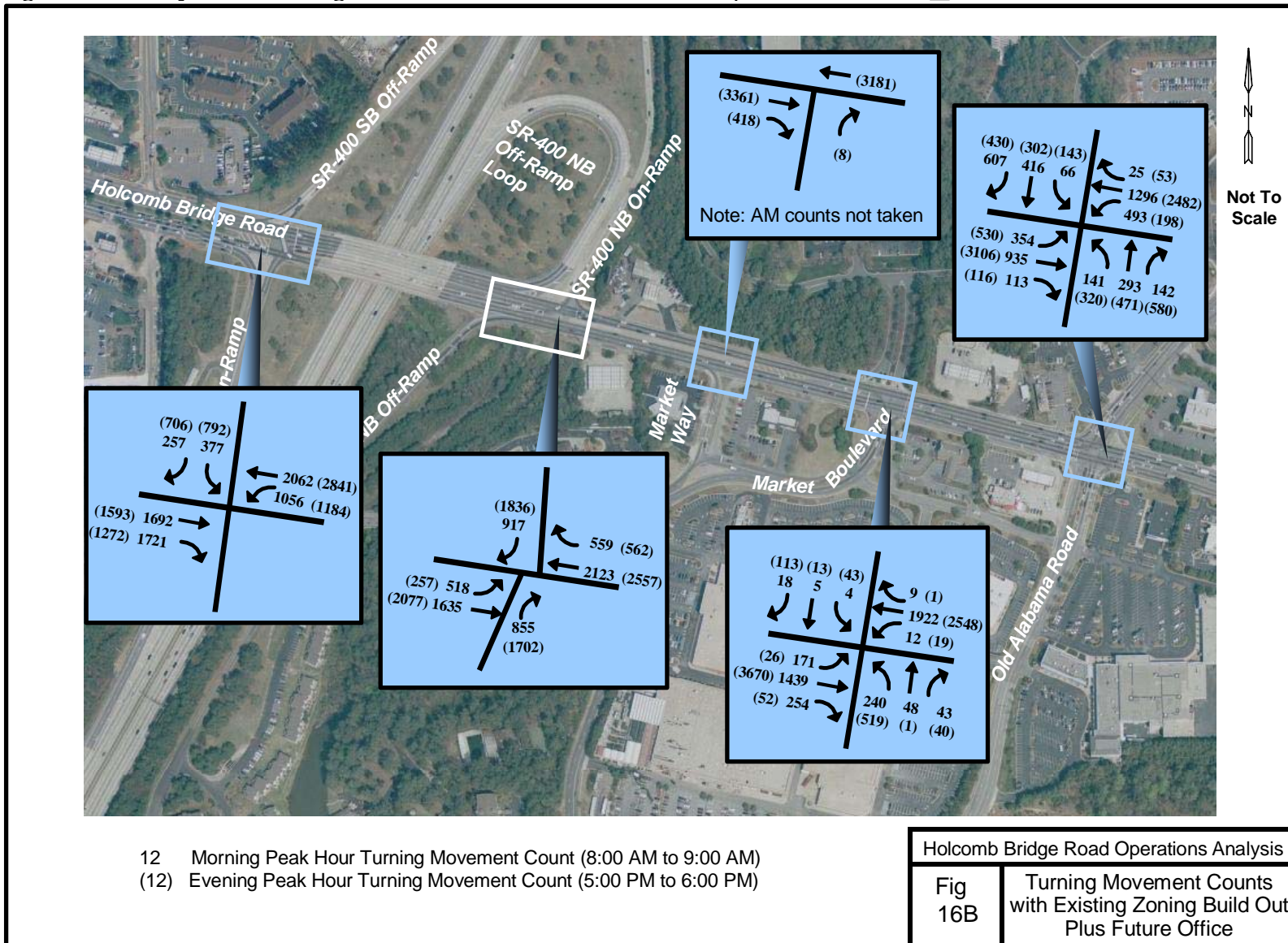
Figure 16A: Turning Movement Counts with Existing Zoning Build Out



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Figure 16B: Projected Turning Counts with Future Office Development



May 4, 2005





### 2.2.4 Comparison of Recent Modifications to Previous Study Recommendations

The traffic conditions resulting from the recent roadway modifications were compared to the previous conditions, before the GDOT modifications and the full set of previous recommendations from the Holcomb Bridge Road Traffic Operations and Safety Analysis, in order to determine the need and effectiveness of potential improvements.

**Table 4  
Comparison of Delay and Level of Service along Holcomb Bridge Road  
Current Conditions versus Conditions Prior to Improvements**

Intersection	AM Peak Hour		PM Peak Hour	
	Delay	LOS	Delay	LOS
<b>Current Conditions</b>				
Holcomb Bridge Road at SR 400 SB	30	C	90	F
Holcomb Bridge Road at SR 400 NB	8	A	45	D
Holcomb Bridge Road at Market Boulevard	29	C	23	C
Holcomb Bridge Road at Old Alabama Road	48	D	141	F
<b>Conditions Prior to GDOT Modifications</b>				
Holcomb Bridge Road at SR 400 SB	28	C	87	F
Holcomb Bridge Road at SR 400 NB	53	D	46	D
Holcomb Bridge Road at Market Boulevard	26	C	27	C
Holcomb Bridge Road at Old Alabama Road	62	E	157	F
<b>1999 Study Recommendations - With Current Volumes</b>				
Holcomb Bridge Road at SR 400 SB	31	C	68	E
Holcomb Bridge Road at SR 400 NB	4	A	44	D
Holcomb Bridge Road at Market Boulevard	22	C	18	B
Holcomb Bridge Road at Old Alabama Road	30	C	119	F

As Table 4 indicates, the current conditions and 1999 study recommendations provide benefits in terms of calculated LOS and delay, however the benefits of these improvements are not fully reflected in the intersection delay calculations. Key improvements address eastbound left-turns spilling into the adjacent through lanes at the critical GA 400 and Old Alabama Road intersections. These improvements allow adjacent through lanes to flow without the previous constraints caused by the spillover traffic. The following is a summary of the findings from this analysis and observations of field conditions.

### 2.2.5 Summary

#### Holcomb Bridge Road at Old Alabama Road

- Recent modifications provide an improvement over prior conditions in terms of handling additional vehicles during the peak hours. The eastbound dual left-turn lanes prevent extensive delays for this movement and prevent spillover of left-turns into the adjacent through lanes. However, a weaving problem has been created on Old Alabama Road as a result of the dual left-turn lanes.
- The full set of improvements recommended in the Holcomb Bridge Road Traffic Operations and Safety Analysis would provide additional benefits over the implemented improvements in regard to intersection delay and continuity of the eastbound through movement. However, these improvements would require significant right-of-way acquisition.
- In addition to the implementation of the original recommendations for the Old Alabama Road intersection, extension of the three lanes eastbound through the Old Alabama Road intersection will be needed to provide LOS conditions better than F (LOS E). Though this additional improvement indicates an improvement to LOS E with current traffic volumes, it is likely that this increase in capacity for the eastbound movement will be filled with traffic rerouting from other corridors. Thus, an overall traffic service benefit would be realized (the intersection would serve more traffic volumes and potentially reduce traffic diverting through neighborhoods), but the delay experienced during the PM peak hour could show only moderate reductions.

#### Holcomb Bridge Road at Market Boulevard

- Although this intersection operates at a good LOS overall, significant queuing and delay are present for the northbound left-turn and southbound right-turn movements at Market Boulevard during the PM peak hour.
- Modifications to provide dual northbound left-turn lanes would reduce this queue and provide additional capacity for future development on the south side of Holcomb Bridge Road (425,000 square-feet of office space for current zoning and additional one million square feet of office space for potential redevelopment).

#### Holcomb Bridge Road at Market Way (right-in and right-out only)

- The intersection of Holcomb Bridge Road at Market Way serves traffic traveling from GA 400 to South Old Alabama Road during the PM peak. Traffic counts indicated 325 vehicles per hour make this movement during the PM peak hour.
- Eastbound traffic on Holcomb Bridge Road shifts right to avoid the upcoming lane drop causing occasional congestion (slow traffic flow) in the through lanes near Market Way.

#### Holcomb Bridge Road at GA 400 Northbound

- The addition of storage length for the eastbound left-turn onto GA 400 North provides a significant improvement for eastbound traffic flow. As a result of the additional storage length, left-turning vehicles no longer spillover to the through lanes, reducing throughput for the eastbound through movement.
- The addition of a westbound right-turn lane at the GA 400 North on-ramp provides additional capacity needed in this critical area in the AM peak hour.
- Despite making the exiting free-flowing northbound 400 ramp onto Holcomb Bridge Road a dedicated through lane beyond Old Alabama Road, queuing still exists on the ramp back onto GA 400 during the PM peak.
- Implementation of pedestrian facilities across the GA 400 interchange was recommended in the Holcomb Bridge Road Traffic Operations and Safety Analysis, and the need for these facilities continues.

#### Holcomb Bridge Road at GA 400 Southbound

- A southbound dual left-turn lane is needed at the GA 400 southbound off-ramp. (As of June 2005 GDOT's planned projects include this improvement.)
- Traffic congestion on GA 400 South often results in traffic backups through the Holcomb Bridge Road interchange during the AM peak hour.
- Implementation of pedestrian facilities across the GA 400 interchange was recommended in the Holcomb Bridge Road Traffic Operations and Safety Analysis, and the need for these facilities continues.
- Operation of the interchange as a diamond with a loop ramp in one quadrant results in continuing LOS deficiencies during the peak hour. Major interchange reconstruction would be needed to significantly improve interchange operations.

### 2.2.6 Aesthetic Improvements

The aesthetic character of the Holcomb Bridge Road corridor is decidedly consistent with a high-speed urban arterial. As a result of design and landscape guidelines established in past years, there are many mature trees and hedges along the corridor that contribute aesthetically. Such guidelines should be continued to insure that future redevelopment contributes to the aesthetic enhancement of the corridor (see Section 5.7 of this report), and landscaping should be added in association with new sidewalk construction that is publicly sponsored and funded.

From a comparison of Holcomb Bridge Road's west side (west of GA 400) with the east side, it is immediately noticeable that the west side includes raised medians with trees and other landscaping whereas the east side does not have landscaped medians. Although current trends in roadway construction typically include landscaped medians on arterials such as Holcomb Bridge Road, that was not the case at the time the road was widened to its current state. There are many opportunities, however, to retrofit the roadway by adding raised landscaped medians that will contribute positively to roadway aesthetics and function. Robert and Company and Day Wilburn Associates have studied the potential for adding medians along the corridor, and it is estimated that over 2.4 miles of median length can be added with minimum conflict. Preliminary discussions with representatives of the Georgia Department of Transportation District 7 Office were favorable towards the addition of medians to the roadway. Additional detail concerning median recommendations is included in Section 5.6.3 of this report.

### 2.2.7 Pedestrian and Bicycle Facilities

The geography of the areas of Roswell located east of GA 400 does not facilitate bicycle and pedestrian mobility. Rather, the nature of the developed environment, notably the area's streets, is often inhospitable to cyclists and pedestrians. Therefore, a concerted and long-term effort will be required to make the area truly bicycle- and pedestrian-friendly.

#### *Existing Bicycle and Pedestrian Facilities*

Approximately 7 miles of bike lanes currently exist in east Roswell. Bikeable shoulders extend along Riverside Drive from Eves Road west under GA 400 and through to Riverside Park. A second bike lane route extends along Martins Landing Road from Riverside Drive to near the Martins Landing community entrance at Holcomb Bridge Road. Additional bike lanes are located on Wileo Road and Steeplechase Drive.

Previously planned additional bike lanes in east Roswell are at various stages of implementation. Funding has been secured for bike lanes to be added to Old Alabama Road from Riverside Drive to Market Boulevard. This project is in the design phase. Additional routes have been previously proposed for Eves Road from Riverside Drive to Holcomb Bridge Road and along the GA 400 right-of-way to connect to the Big Creek multi-purpose trail. These two routes will provide

important connections to existing recreation facilities and they should remain the top short-term priorities. Additional bike lanes for Wileo Road are also in the design phase.

Sidewalks currently exist along many street segments in the east Roswell area. However there is not a comprehensive, connected system of sidewalks. Along Holcomb Bridge Road between GA 400 and the Chattahoochee River, there are approximately 2.65 miles of existing sidewalk. In the same area, Robert and Company surveys indicate that there are approximately 7 miles of “sidewalk gaps” or areas of roadside without sidewalk. These estimated sidewalk needs have been organized according to the three identified nodes and “internodal” areas as indicated in the following table.

**Table 5  
Holcomb Bridge Road Sidewalk Inventory**

Corridor Node	Sidewalks (Feet)	
	Existing	Proposed
GA 400/Old Alabama	4,636	2,844
GA 400/Old Alabama to Centennial	2,334	5,498
Centennial	2,243	6,303
Centennial to Nesbit Ferry	580	6,221
Nesbit Ferry	2,532	5,743
Nesbit Ferry to Chattahoochee	1,635	8,230
Total	13,961	34,839

In a major corridor environment such as Holcomb Bridge Road, one of the most significant challenges to cyclists and pedestrians is crossing the road. The signalized intersections on Holcomb Bridge Road generally are equipped with crosswalks and pedestrian crossing signals. However, additional measures to facilitate road crossing may be appropriate at some locations. In coordination with the East Roswell Bicycle and Pedestrian Connectivity Study being conducted by the city’s recreation department, traffic engineers and transportation planners with Day Wilburn Associates (DWA) analyzed the feasibility of a number of the corridor’s key intersections for safe and functional at-grade bicycle/pedestrian crossings. The following Holcomb Bridge Road intersections were analyzed:

- Market Boulevard
- Old Alabama Road
- Holcomb Woods Parkway
- Martin’s Landing Drive
- Eves Road,
- Fouts Road
- Nesbit Ferry Road

Based on traffic engineering analysis, there is a potential at-grade crossing in each of the three activity nodes. These locations are Holcomb Bridge Road and Holcomb Woods Parkway, Holcomb Bridge Road and Fouts Road and Holcomb Bridge Road and Nesbit Ferry Road.

Where at-grade crossings are viable, intersection improvements to facilitate at-grade crossing should be constructed in association with trails linking to those locations. Improvements at Fouts Road, in particular, are recommended due to the potential to establish a better connection between East Roswell Park and Centennial High School.

In addition to at-grade crossing analysis, further feasibility study was conducted concerning grade-separated crossing possibilities along the corridor. Anticipated to be the highest priority is the western end of the Holcomb Bridge Road corridor, from GA 400 to Holcomb Woods Parkway, where there is a heavily developed and congested corridor environment. The best location in this area for a grade-separated crossing is along the GA 400 right-of-way. A second location studied for feasibility of grade-separated crossing is the vicinity of the intersection of Eves Road with Holcomb Bridge Road. At this Eves Road location, the most feasible alternative is for construction of a bicycle/ pedestrian bridge over Holcomb Bridge Road coordinated with and integrated into future land development projects on either side of Holcomb Bridge Road. The third potential location for grade-separated crossing, near the entrance of the Martins Landing community, is somewhat feasible due to City-owned property/right-of-way that exists on the north and south sides of Holcomb Bridge Road. However, existing overhead utilities pose a conflict with a bridge structure, and, on the north side of Holcomb Bridge Road, there is not a readily available connection beyond the City park.

## 2.3 Demographics & Market Overview

To provide the market and economic context necessary to evaluate the potential of the Holcomb Bridge Road East corridor, an overview of the Roswell market and an assessment in terms of the three key demand segments - retail, office, and residential was completed. This overview provides a “snapshot” of demographic, economic and real estate conditions in the study area and was used to suggest topics for more detailed research in later phases of the study

### 2.3.1 Demographics

In the spring of 2004 the study area contained approximately 28,792 residents, which represents 36.5% of Roswell's population. The residents of the study area are well educated (55.4% college graduates) and 50.6% work in professional or managerial positions

Median household incomes in the study area are considered are high \$85,931 versus \$83,702 for City of Roswell as a whole, but is slightly lower than that of the North Fulton area; \$87,843. Overall the area's median income is 21.7% above the Atlanta regional median of \$68,800 and only 23.4% of households in study area have incomes below \$50,000.

### 2.3.2 Housing

Single-family detached housing dominates the study area at 57.7% of the total housing, but less so than in the City of Roswell or the North Fulton areas. Due to the predominance of single family detached housing it is not surprising that housing tenure in the study is skewed towards owner occupancy (63.6% of households) versus renter occupancy (36.4% of households). Half of the single family housing in study area was built in the 1980's and only 28% was constructed during the 1990s. This makes housing along the corridor older than the larger North Fulton area where 46.9% of the housing was constructed in the 1990's. The average value of homes in the study area is \$243,553 and 12% of homes are valued over \$500,000. Some of the most recent construction in the area has been in the multi-family housing market, with permits for these developments accounting for 27.5% of the building permits drawn during the 2000 – 2003 time period. In total there are nine rental communities with 3,044 units located in the study area. Occupancy levels in these communities are high with all but one 15 year old apartment complex having 91.1% occupancy. Resales accounted for most (85% – 95%) of housing unit sales in the corridor during the last three years (2001 – 2004), with the largest number of sales in \$150,000-\$199,000 range. Attached housing accounted to between 20% and 27% of housing sales during this period. The pace of housing sales in the study area declined in each of the last three years.

### 2.3.3 Employment

There are 821 businesses within the study area. 11,853 people work in the area, this number has increased by 77% in the last six years. The jobs located in the study area

account for 31% of Roswell's job base. The largest numbers of jobs along the corridor are in the services and retail sectors. Incidentally, employment statistics show a strong "manufacturing" presence due to the Kimberly Clark offices located just off GA 400 on Holcomb Bridge Road. However, this is a research and development facility and not actually used for manufacturing.

#### 2.3.4 Retail & Office Markets

The office sector in the area is considered small, consisting of 585,904 square feet and representing 4% of the North Fulton sub market. The vacancy rate is 21% (or 129,335 sq. ft.) a reflection of the general softness in the North Fulton sub market. The retail sector of the corridor is also experiencing high vacancy rates, of the 1,411,000 square feet of retail space distributed amount 12 major retail centers in the study area 29% was vacant at the time of analysis (Summer 2004). This is despite the \$472,000 of retail expenditures generated by the households in the area, which account for 37% of Roswell's retail potential.

#### 2.3.5 Conclusions

General conclusions that can be drawn from the review of area demographics and a physical review of the retail, housing and office establishments along the corridor area as follows. The study area is multidimensional older suburb: it's greatest growth was experienced during the 1980 – 1995 time period. There is a significant single-family "neighborhood" housing feel that is complimented by a strong multifamily market with a mix of housing options. Commercial development is concentrated at key intersections separated by significant existing green buffers. Despite the affluent, well educated, mix of household types and sizes, there are high vacancies in office and retail sectors and significant tenant turnover in the retail market. Some causes of this may be the overabundance of retail space in the area, which has 15% more retail square footage per resident than the larger North Fulton sub market, and 25% more per resident than experienced in the Atlanta metropolitan region as a whole. However, there has been recent investment in the area and several "new urbanist" developments (Ellard, Centennial) have come on line in recent years showing the emergence of second-generation development patterns in the area.



## **CHAPTER 3**

### *PUBLIC PARTICIPATION*

- 3.1 Overview
- 3.2 Steering Committee
- 3.3 Public Workshops
  - 3.3.1 Project Kickoff Meeting
  - 3.3.2 Community Design Charette
- 3.4 Stakeholder Interviews
  - 3.4.1 Homeowners Group Comments
  - 3.4.2 Business/Commercial Comments
  - 3.4.3 Development Community Comments

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### 3.1 Overview

The Public Involvement Plan for the Roswell Holcomb Bridge East Revitalization Plan involves interface with an appointed Steering Committee (in coordination with the Roswell Redevelopment Task Force) and with the general public including residents, business representatives, institutional representatives and others. The following is a description of the involvement of the Steering Committee and the public during the study process.

### 3.2 Steering Committee

The Steering Committee for the Holcomb Bridge Road East Revitalization study included members and representatives of many of the corridor's homeowner's associations, the management companies of the large shopping centers located along the corridor, the city's community development department, the city's redevelopment task force and members of City Council.

The Steering Committee provided input and guidance concerning issues to be addressed and ways to engage the public during the planning process and assisted consultants in the analysis and "listening" process. This group was also responsible for reviewing draft goals, policies, concepts and recommendations in the plan and provided input for revisions. It is the hope that the Holcomb Bridge Road East Revitalization Study Steering Committee will continue to move the study forward by becoming outspoken advocates for the Holcomb Bridge East Revitalization Plan and working for implementation of its recommendations.

### 3.3 Public Workshops

Involvement of the general public was garnered through three carefully planned public presentation and workshop events, as described below:

#### 3.3.1 Project Kick-Off Meeting

The workshop began with a presentation of the purpose and scope of the planning project. Following this Ken Bleakly provided a short overview of the corridor's demographics and state of the retail, office and residential environments. After this presentation a Retail Shopper Survey was distributed to the meeting attendees and there was discussion of how the survey would assist the study team in ascertaining retail needs and desires of the population served by the retail outlets along the Holcomb Bridge East corridor. A copy of the survey and a summary of the results are provided in Appendix 1. The results of the survey have been incorporated into redevelopment recommendations.

Following the Shopper Survey, the consultant team led the attendees through a Visual Character Preference Survey. This survey consisted of an interactive display of a series of 100 images of the built environment, including images from Roswell and other locations. Survey participants were asked to use the score sheets provided to rate each image on a scale from -3 to +3 based solely on individual preference for the visual character of development shown in the image. The top ten and bottom ten images from the survey are shown on the following pages. A text summary of the survey results is provided in Appendix 2.

### Bottom Images from Preference Survey

*Images are ranked in order from lowest to highest negative score.*





-7



-8



-9



-10

Top 10 Images from Preference Survey  
*Images are ranked in order from highest to lowest positive score.*



1



2



3



4



5



6



Following the Preference Survey, meeting attendees were invited to share their thoughts and comments on the existing conditions and future redevelopment of the East Holcomb Bridge Road corridor during a “Town Hall” discussion facilitated by the consultant team. A summary of the comments gathered during this discussion is provided in Appendix 3.

### 3.3.2 Community Design Charette

A second interactive workshop was held on July 13, 2004. This meeting began with a presentation of the results of the Visual Character Preference Survey and Shopper Surveys conducted during the first public meeting followed by a discussion of concepts and key preliminary recommendations for the Holcomb Bridge Road East Corridor.

After the presentations, the meeting attendees were divided into small groups and asked to study base maps of the three defined nodes within the Holcomb Bridge Road East Corridor; the GA 400 / Old Alabama Node, Centennial / Eves Road Node, and the Nesbit Ferry Node. The base maps included aerial photos of the nodes and depictions of the existing uses, current zoning, building locations and transportation networks. Participants were provided with color-coded paper squares with images representing a variety of development types and asked to place them where they felt appropriate on the base maps. Participants were also asked to identify areas needing redevelopment improvement (such as older strip shopping centers), to delineate areas for the preservation of existing land uses (such as the stable single-family neighborhoods), and to identify areas appropriate for new development.

In addition to mapping recommendations, each group was given the opportunity

to provide comments regarding the challenges and opportunities in the study area. A summary of group comments is provided in Appendix 4. The maps generated by the charette groups, combined with information gathered during the assessment and analysis phases of the study process, were reviewed by the consultant team and steering committee to produce a Conceptual Master Plan for the Holcomb Bridge Road East Corridor.

### 3.4 Stakeholder Interviews

Consultants and city economic development staff conducted a series of interviews during the week of May 24, 2004. Interviewees included representatives of homeowners groups in the corridor's large subdivisions; Martins Landing, Horseshoe Bend, Willow Springs, and Ellard); a resident-at-large from the corridor, business/commercial property owners representatives and developers. (A list of stakeholders interviewed and a summary of their comments are included in Appendix 5)

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# **CHAPTER 4**

## *MARKET ANALYSIS*

4.1 Background and Analysis Assumptions

4.2 Key Observations

4.2.1 Old Alabama / GA 400 Node

4.2.2 Centennial Node

4.2.3 Nesbit Ferry Node

4.3 Summary of Redevelopment Scenarios

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## 4.1 Background and Analysis Assumptions

The market analysis for the development and redevelopment of the three identified activity centers along the Holcomb Bridge Road corridor was prepared with two overriding assumptions. First, to economically redevelop a parcel, sufficient project value must be created to justify purchasing the property for redevelopment purposes. Tax values typically lag actual market values. Market values are subject to wide variation based on different owner motivations, recent leasing activity and a multitude of other factors. Therefore, tax values provide the baseline for this analysis. One or more redevelopment scenarios were prepared for each potential redevelopment parcel within the identified activity nodes. Second, due to the study area's over supply of retail space, an overriding factor in the redevelopment scenarios developed was the goal of minimizing the amount of retail in the three activity areas and providing a mix of uses that could generate sufficient revenues to justify the land costs. A variety of mixed-use concepts were explored such as adding office space and/or residential units to the activity nodes. Again, the overall goal was the creation of fiscally viable redevelopment scenarios that reduced the amount of retail space in the corridors activity nodes.

## 4.2 Key Observations

Land in the Holcomb Bridge Road East Corridor is expensive. While the area has been experiencing a transition in its retail uses and high vacancies, values in the area are significant, which creates a substantial financial hurdle for redevelopment. To make the planned mixed-use strategy work requires the ability to pay substantial sums for the potential sites. When evaluating potential sites for redevelopment, a ratio of the land after development to the current value of the site was used to determine financial feasibility. A ratio of 1.0 is needed for a redevelopment project to pay land prices equal or greater than the current tax value of the project; this is minimal threshold to make the project economically viable.

### 4.2.1 Old Alabama / GA 400 Node

Several of the sites combine high values with small size and therefore do not appear economically feasible for redevelopment; notably the Holcomb Bridge Strip Retail and possibly the Holcomb 400 site. Furthermore, these projects are performing adequately to command high land values, which makes them difficult to redevelop.

Current city zoning policies limit mixed-use densities to a maximum of 8 units to the acre. At that density it is not economically feasible to create a residential mixed-use project on any of the GA 400/Old Alabama sites evaluated. To make a mixed-use redevelopment with a residential component financially feasible it will require increased residential densities suitable for a mix of townhouses and condominium flats. As an alternative to mixed use with this type of residential component, mixed-use



developments with a significant office component could be economically viable, once the area's office market overcomes its current weakness.

One redevelopment approach for this area could be to provide streetscapes and infrastructure funding to support the gradual transition of these retail areas to more pedestrian-friendly mixed use. TAD (tax allocation district) financing could also be employed to close the financial gap to make redevelopment happen

Allowances for limited amounts of residential development, with densities of 12-16 units per acre could be used to revitalize the area. This incentive could be achieved through revisions to the city's existing OCMS zoning category and made available on a first-come/first-served basis to major developments in the activity area. By incorporating limits on the total number of residential units allowed in the activity center only one or two sites would be allowed to redevelop at the higher densities. The result would be only modest increases in development in the activity area while creating the economic incentives to create a town center and removing some of the existing excess retail inventory.

The GA 400/Old Alabama activity area is seeing some of the problems caused by severe retail vacancy resolving itself as property owners work diligently to re-tenant their spaces in recognition of the market shift from region-serving to neighborhood/convenience retailing. Recent decisions by Publix, Home Depot and the former Cub Foods site indicate positive changes are occurring and will likely continue to occur in the retail make-up of the corridor.

#### 4.2.2 Centennial Node

In the Centennial activity area, a site that offers potential for a future conservation subdivision development which will help create a major greenway along Holcomb Bridge Road was assessed. For the conservation residential parcel we assumed that 24 acres remain as green space and buffer. The remaining 25 acres could be developed as either cluster single-family housing or detached single-family housing. Given the varied terrain of the site a more detailed analysis of its development capacity should be undertaken to determine if even this density of development is possible.

As noted in Section 2.1.1, there is also a 28 acre undeveloped tract on the north side of Holcomb Bridge Road in this node. This sit is zoned for office development, the construction of which will add a missing component to the mix of uses currently present in the Centennial area.

#### 4.2.3 Nesbit Ferry Node

The Nesbit Ferry activity center was analyzed as a possible location for senior housing and medical uses. A project the Tree Ridge property and surrounding parcels could generate sufficient revenues from various combinations of senior housing and medical office development to be economically viable for redevelopment. This redevelopment

scenario would add a type of residential unit not currently present on the corridor. This would enhance the perception of the corridor as a lifecycle community, offering the existing rental housing for younger households, single-family housing, which appeals to families, and options for seniors to remain in the community through independent living units and assisted living units.

#### 4.3 Summary of Redevelopment Scenarios

A number of schedules describing possible redevelopment scenarios for the three activity centers proposing for the Holcomb Bridge East corridor: GA 400 / Old Alabama Road, Centennial, and Nesbit Ferry are included in Appendix 6. Included here is a summary sheet for eight parcels identified in the three activity centers (Table 6) that have the greatest redevelopment potential. This analysis is based on a review of Fulton County tax records to determine the current tax value of each of the potential redevelopment areas.

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Table 6: An Economic Analysis of Redevelopment Scenarios for Activity Centers

NODE Site Project	Acreage	Current Land Use		Redeveloped Value					Ratio New Land Value to Taxable
		Taxable Value		Residential		Overall	Project	Land	
		Total	Per Acre	Units	Per Acre	FAR	Value	Value	
<b>GA 400/OLD ALABAMA</b>									
East Roswell Center*	33	\$ 12,350,700	\$ 374,264						
Option A: Commercial Town Center				66	2	0.14	\$ 68,200,000	\$ 13,460,000	1.09
Option B: Residential Town Center				264	8	0.13	\$ 67,800,000	\$ 13,560,000	1.10
Expanded Kings Market Site	22	\$ 23,100,000	\$ 1,067,301						
Option A: Vertical Mixed Use				396	18	0.53	\$ 116,160,000	\$ 23,232,000	1.01
Option B: Horizontal Mixed Use				308	14	0.42	\$ 120,120,000	\$ 23,232,000	1.01
Option C: Residential Mixed Use				484	22	0.51	\$ 119,900,000	\$ 23,980,000	1.04
Holcomb 400	12	\$ 10,557,900	\$ 894,737						
Option A: Mixed Use				192	16	0.86	\$ 54,120,000	\$ 10,824,000	1.03
Option B: Residential Mixed Use				240	20	0.79	\$ 55,000,000	\$ 11,000,000	1.04
Holcomb Bridge Strip Retail	6	\$ 6,532,500	\$ 1,088,750						
Option A: Mixed Use				132	22	0.84	\$ 33,000,000	\$ 6,600,000	1.01
Option B: Residential Mixed Use				144	24	0.74	\$ 33,880,000	\$ 6,776,000	1.04
Charter School Site	9	\$ 6,947,400	\$ 771,956						
Option A: Mixed Use				135	15	0.62	\$ 35,980,000	\$ 7,160,000	1.03
Option B: Residential Mixed Use				144	16	0.52	\$ 34,980,000	\$ 6,996,000	1.01
Expanded Holcomb Woods	35	\$ 24,370,000	\$ 687,646						
Option A: Mixed Use				420	12	0.37	\$ 122,860,000	\$ 24,572,000	1.01
Option B: Residential Mixed Use				490	14	0.034	\$ 135,600,000	\$ 25,860,000	1.06
<b>CENTENNIAL ACTIVITY AREA</b>									
Conservation Residential*	49	\$ 880,000	\$ 120,000						
Option A: Cluster Single-Family				50	1	0.1	\$ 17,500,000	\$ 3,500,000	1.17
Option B: Single-Family Detached				25	0.5	0.06	\$ 15,000,000	\$ 3,000,000	1.00
<b>NESBIT FERRY ACTIVITY CENTER</b>									
Expanded Tree Ridge Site	44	\$ 22,079,000	\$ 512,630						
Option A: Senior Independent Living				616	14	0.32	\$ 118,800,000	\$ 23,760,000	1.08
Option B: Medical Office/Senior Housing				528	12	0.24	\$ 122,100,000	\$ 23,100,000	1.05
Option C: Senior Continuum of Care				704	16	0.3	\$ 115,280,000	\$ 23,056,000	1.04
Note: * Due to low tax value of undeveloped land parcels, market values were estimated									
Source: Fulton County Assessors, Bleakly Advisory Group									

# **CHAPTER 5**

## *MASTER PLAN & RECOMMENDATIONS*

### 5.1 Overview

### 5.2 GA 400 / Old Alabama Node

- 5.2.1 Future Character and Goals
- 5.2.2 Revitalization Recommendations
- 5.2.3 Transportation Improvements
- 5.2.4 Summary Redevelopment Recommendations

### 5.3 The Centennial Node

- 5.3.1 Future Character and Goals
- 5.3.2 Revitalization Recommendations
- 5.3.3 Summary Redevelopment Recommendations

### 5.4 Nesbit Ferry Node

- 5.4.1 Future Character and Goals
- 5.4.2 Revitalization Recommendations
- 5.4.3 Summary Redevelopment Recommendations

### 5.5 Inter-Nodal Areas

- 5.5.1 Future Character and Goals
- 5.5.2 Revitalization Recommendations

## 5.1 Overview

The study area for the Holcomb Bridge Road East Revitalization Study is an auto-oriented corridor containing residential, commercial and civic uses with little reference to one another. This development pattern is typical of suburban areas throughout the metropolitan Atlanta region and nationwide, however it is becoming increasingly evident that auto centric design may have negative impacts on the social and economic health of communities.

As outlined in preceding sections these impacts are apparent throughout the Holcomb Bridge Road Corridor. The retail vitality of the corridor has suffered as many shoppers have come to prefer newer more walkable shopping centers such as The Avenues. Additionally, despite the significant civic investments in the community, such as schools and parks, the study area lacks a distinct character or a sense of place that is cited as a key component of many successful suburban activity centers.

These findings have led to the development of a vision for a revitalized Holcomb Bridge Road corridor that focuses on stimulation, connection and diversification of the area.

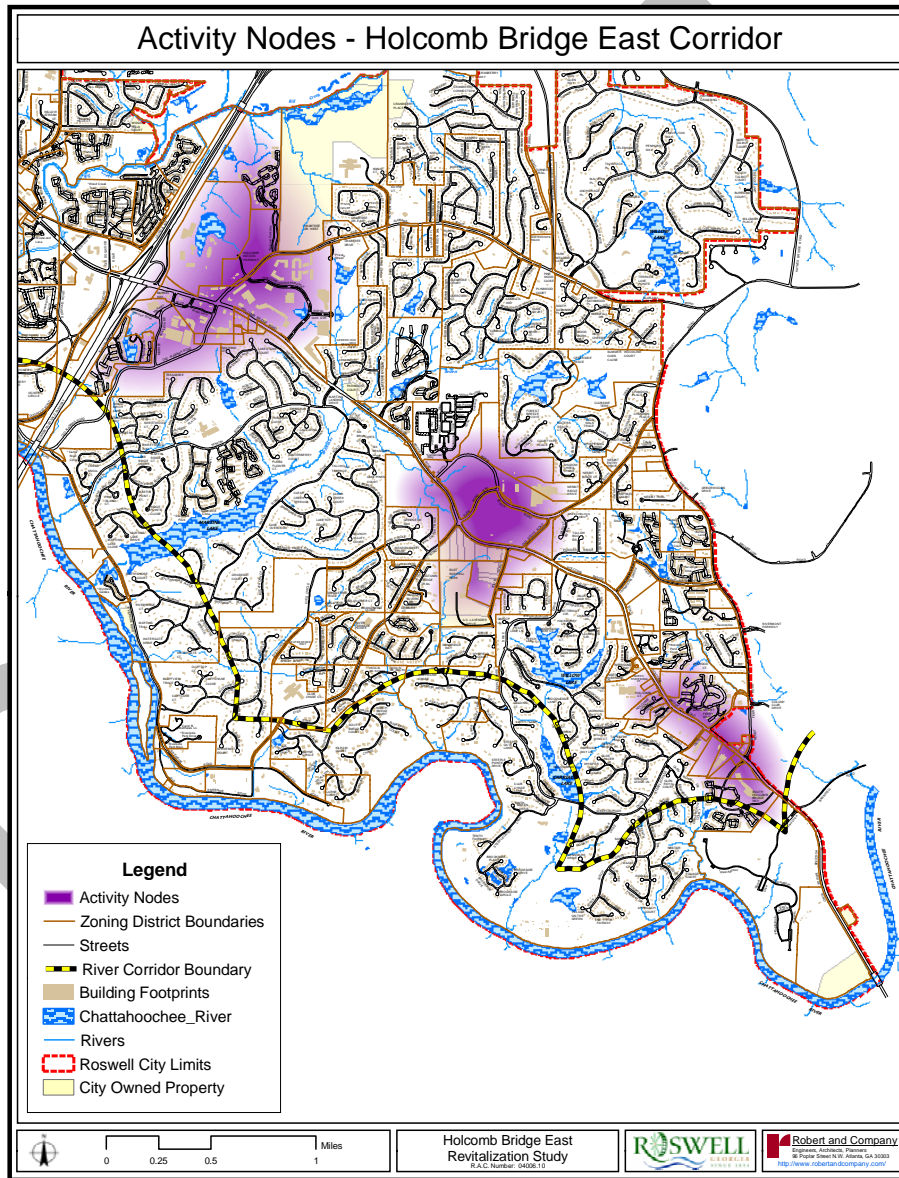
*Holcomb Bridge Road is the east – west corridor at the center of Roswell: A community, which provides a desirable, sustainable, safe and manageable environment for living, working and recreation that is inclusive and supportive of the city's diverse mixture of neighborhoods, historic and cultural resources and commercial developments.*

As expressed in the Vision Statement and discussed in the existing conditions section of this document, the study area is diverse and cannot be adequately addressed as one place. This assertion is supported by the fact that people will generally limit pedestrian activity to areas of a ¼ mile or less. Therefore, the structure for the revitalization plan of the Holcomb Bridge Road East corridor has been established as a nodal-based planning structure. This means that along the entire length of the corridor (5.1 miles), the emphasis for revitalization planning is placed on enhancing and repositioning uses in identified areas of activity concentration.

The Master Plan highlights the potential to develop three distinct activity centers (Figure 17) along the Holcomb Bridge Road East corridor. These centers include: GA 400 / Old Alabama Road, a medium to high density mixed-use retail and employment core for the corridor focused on multi-modal transportation options and the redevelopment of ageing commercial centers and apartment complexes. Centennial, a civic hub for the east Roswell community surrounding the intersections of HBR with Eves and Fouts Roads and Scott Boulevard that provides connections between the corridors large residential neighborhoods and schools and recreational amenities. Nesbit Ferry, a moderate density mixed-use area centered on the redevelopment of large multi-family housing complexes in to a diverse mix of senior housing providing a continuum of care allowing area residents to age in place.

Specific planning recommendations have been developed for each nodal area including development types, land use patterns and transportation improvements. Likewise, there are improvements needed in the areas between the corridor's nodes for the purpose of maintaining the character of these activity areas and preserving their integrity and ability to function as nodes. The following sections include recommendations for projects and policies to achieve the revitalization vision of an integrated, diverse and vital Holcomb Bridge Road Corridor.

Figure 17: Identified Corridor Nodes



## 5.2 GA 400 / Old Alabama Node

### 5.2.1 Future Character and Goals

The area surrounding the intersections of Holcomb Bridge Road with GA 400 and Old Alabama Road is the retail and employment center of east Roswell. However, the existing commercial centers, such as King's Market and Holcomb Woods, were designed almost exclusively for automobile access and they lack the human-scale and character that are desirable in retail centers today (such as The Avenues developments in Cobb and Gwinnett, which were referenced often in the study process). The autocentricity of the node is increased by the separation of retail and business uses in disparate centers lacking direct connections.

In the future, the GA 400/Old Alabama node should continue to serve as a community business center and accommodate major office and retail land uses. The node should be enhanced through the introduction of appropriate mixed-use configurations, design standards and transportation facilities that promote a easy circulation through a multi-modal environment.

### 5.2.2 Revitalization Recommendations

Based on analysis of current land values, existing commercial center properties in the GA 400/Old Alabama area are so expensive that they do not appear likely candidates for redevelopment in the short term. An appropriate approach for this area will be to provide streetscapes and infrastructure funding to support the gradual transition of these retail areas to more pedestrian-friendly mixed-use environment.

In order to hasten redevelopment of this node's commercial centers in a manner that reduces retail square footage, a stated goal of the city during the revitalization study process, the City could consider allowing development of a limited amount of residential mixed-use. This incentive could be achieved through modifying the city's existing OCMS zoning category. However, modifications should include limits on the total units and square footages or percentage mix of residential development allowable in the district. This approach would result in only modest increases in development in the activity area while creating the economic incentives for revitalization and removing some of the existing excess retail inventory.

In the short-term (0 – 5 years), the City of Roswell should continue to assist area property managers and owners in their efforts to attract quality tenants to fill vacant spaces in commercial centers. Market forces are currently assisting with this objective due to the attractiveness of the local market conditions. The current Publix and Home Depot development activity, among other examples, is evidence of the local market's attractiveness. Demographic statistics and the retail market analysis conducted as part of this planning process provide additional evidence that the market should be attractive to a wide variety of retailers, including very strong support for certain

businesses such as restaurants. This retail market analysis, providing evidence of strong demographics, customer support and overall market strength, should continue to be used as a tool to recruit new businesses to the area. Also in the short-term, the City of Roswell should accomplish improvements to public streets and rights-of-way in the GA 400/Old Alabama Node. Some opportunities exist for infill development within the short-term, and such development should contribute to the long-term goal of a mix of uses and pedestrian-oriented environment.

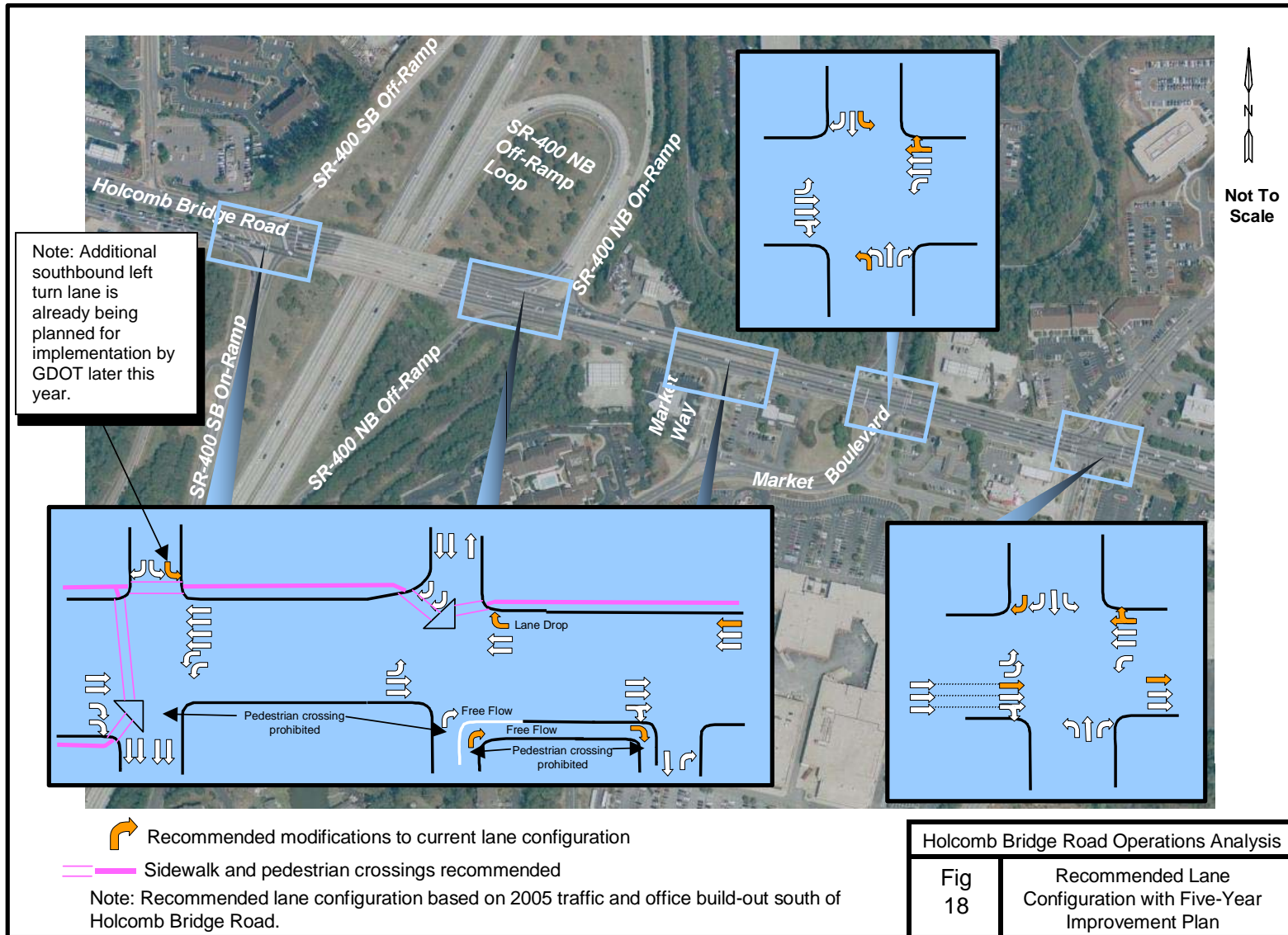
In the long term (5+ years), the goal of redeveloping and revitalizing the area's commercial centers should be pursued. Making limited adjustments to the city's existing OCMS zoning classification could provide a valuable redevelopment alternative and become economically viable over time. Also appropriate for the mid- to long-term (due to current market conditions) is the development of a mixed-use office center along Old Alabama Road south of Holcomb Bridge Road. In this area, the existing multi-family residential property (Raintree Apartments – zoned R-4) should be incorporated into an office/mixed-use redevelopment concept. These large-scale projects should help achieve a true mixed-use, multi-modal environment to serve the local population with employment opportunities, retail services, residential options and recreational amenities

### 5.2.3 Transportation Improvements

Based on the existing conditions analysis potential intersection and roadway improvements for the GA 400 / Old Alabama Node were identified and tested to determine their effectiveness in reducing congestion based on 2005 traffic volumes and additional development assumptions. The improvements proved most effective in providing conditions better than LOS F for the intersections in the GA 400 / Old Alabama node are recommended here for implementation. Figure 18 shows the recommended lane configuration and Figure 19 shows the recommended guidance signage modifications.

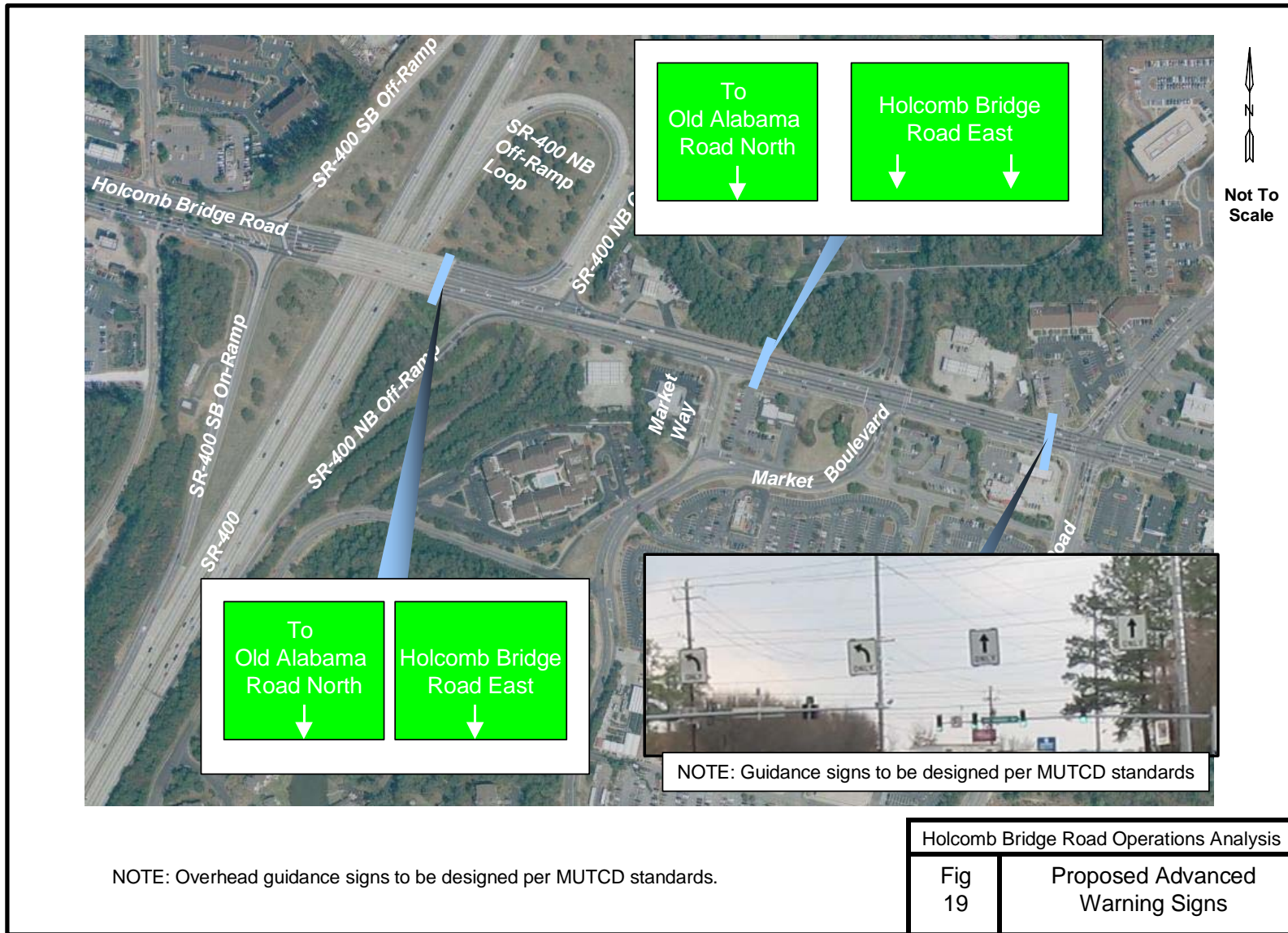


Figure 18: Recommend Short Term Lane Configuration



May 4, 2005

Figure 19: Recommended Signage



May 4, 2005

### *Recommended Short Term (0 – 5 Year) Improvements*

- The improvements indicated below are recommended for implementation in the short-term (five-year improvement program) to address current needs. Due to apparent limited impacts, implementation of these improvements is recommended in the early part of the program.
- Provide additional overhead guidance signage along Holcomb Bridge Road to indicate lane assignment for eastbound traffic to facilitate completion of lane changes prior to Market Boulevard (see Figure 19).
- Modify GA 400 southbound off-ramp to provide dual left turn lanes. (This improvement is already being planned by GDOT for implementation later in 2005.)
- Extend the westbound right turn lane on Holcomb Bridge Road at GA 400 to the Market Boulevard/Kimberly Clark Driveway. Provide a southbound free-flow right turn lane at Market Boulevard to flow into this extended right turn lane.

In addition to the immediate implementation improvements indicated above, the following improvements are recommended for implementation as a part of the five-year implementation plan:

- Construct a second northbound free-flow lane from GA 400 northbound to connect directly to Market Way. Install appropriate signage along the GA 400 off-ramp to indicate travel to Market Boulevard and Old Alabama Road.
- Provide a pedestrian crossing of GA 400 along the north side of Holcomb Bridge Road:
  - Add sidewalk along the north side of the bridge with fencing along the outer edge of the bridge to protect pedestrians from the drop-off to GA 400.
  - Install a pedestrian signal phase across the GA 400 northbound off-ramp/on-ramp. The vehicular movement would be stopped at the appropriate point in the signal cycle based on pedestrian actuation.
  - Install a pedestrian signal phase across the GA 400 southbound off-ramp.
  - Install a pedestrian signal phase across Holcomb Bridge Road on the west side of the GA 400 southbound ramp intersection.
  - Install a pedestrian signal phase across the GA 400 southbound on-ramp from Holcomb Bridge Road eastbound including a new signal phase to stop the two free-flow eastbound dual right turn lanes. The vehicular movement would be stopped based on pedestrian actuation.
  - Prohibit pedestrian crossing of GA 400 along the south side of Holcomb Bridge Road.
- Construct improvements to emphasize use of Market Boulevard for access south of Holcomb Bridge Road to/from the west.
  - Modify Market Boulevard northbound approach to provide dual left turn lanes with protected signal phasing.

- Provide Market Boulevard to Old Alabama Road as a multi-lane public road connection to minimize effects of development on Holcomb Bridge Road at Old Alabama Road intersection.
- Maintain existing northbound green time at Old Alabama Road (do not increase with additional development) to encourage use of Market Boulevard for access to Holcomb Bridge Road.
- Modify the existing drop-lane configuration by constructing an additional left turn storage lane for the Holcomb Bridge Road eastbound approach from Market Boulevard to Old Alabama Road. An additional improvement will continue the third eastbound through lane (that will then no longer be a dropped lane) past Old Alabama Road where the three through lanes will merge to two through lanes.
- Provide a second southbound right turn lane for the Old Alabama Road approach to Holcomb Bridge Road. Operate as a dual right turn movement with a protected signal phase.
- Provide an additional westbound through lane from east of Old Alabama Road to GA 400. Connect to the westbound right turn lane extended from GA 400 North ramp to Market Boulevard as an immediate implementation item.

As discussed previously, the current level of traffic congestion suggests demand is likely to exceed the volumes that able to be served by the intersections during the peak hours. Therefore, improvements in intersection operations may result in some trips rerouting to the Holcomb Bridge Road corridor or concentrating their arrival times, resulting in delay reductions lower than those projected in the analysis, because the roadway will be serving additional traffic volumes in the peak hour. Table 7 shows the calculated intersection LOS with the current volumes and volumes with additional office development assumptions south of Holcomb Bridge Road.

**Table 7**  
**Comparison of Delay and Level of Service along Holcomb Bridge Road**  
**Current Conditions and Recommended Improvements**

Intersection	AM Peak Hour		PM Peak Hour	
	Delay	LOS	Delay	LOS
<b>Current Conditions</b>				
Holcomb Bridge Road at SR 400 SB	30	C	90	F
Holcomb Bridge Road at SR 400 NB	8	A	45	D
Holcomb Bridge Road at Market Boulevard	29	C	23	C
Holcomb Bridge Road at Old Alabama Road	48	D	141	F
<b>Conditions with Recommended Lane Configuration - Current Volumes</b>				
Holcomb Bridge Road at SR 400 SB	31	C	68	E
Holcomb Bridge Road at SR 400 NB	15	B	26	C
Holcomb Bridge Road at Market Boulevard	15	B	32	C
Holcomb Bridge Road at Old Alabama Road	28	C	38	D
<b>Conditions with Recommended Lane Configuration - Development Volumes Existing Zoning</b>				
Holcomb Bridge Road at SR 400 SB	37	D	70	E
Holcomb Bridge Road at SR 400 NB	21	C	27	C
Holcomb Bridge Road at Market Boulevard	19	B	42	D
Holcomb Bridge Road at Old Alabama Road	34	C	54	D
<b>Conditions with Recommended Lane Configuration - Development Volumes Future Zoning</b>				
Holcomb Bridge Road at SR 400 SB	44	D	76	E
Holcomb Bridge Road at SR 400 NB	23	C	27	C
Holcomb Bridge Road at Market Boulevard	19	B	64	E
Holcomb Bridge Road at Old Alabama Road	46	D	86	F

The flow of cyclists and pedestrians within and through this node is key to creating a truly multi-modal environment. Additionally, the GA 400 / Old Alabama Road node lies between two regional bicycle and pedestrian trail, the Riverside Drive/Chattahoochee River Trail network to the south and Big Creek Trail to the north. As traffic engineering analysis showed, the heavy automobile traffic in the node strictly limits the locations where Holcomb Bridge Road can be crossed safely. On the eastern end of the node, traffic engineering analysis showed that the corridor's intersection with Holcomb Woods Parkway affords an at-grade crossing opportunity. However, this crossing location does not provide connectivity for the two trail networks.

The East Roswell Bicycle and Pedestrian Trail Connectivity Study, which is currently being completed by the city's recreation department provides an analysis of the potential for a grade-separated crossing of HBR within the GA 400 / Old Alabama Node. The study has identified the area east of the GA 400 access ramps as the most practical and appropriate location for this crossing. Preliminary study recommendations identify two crossing options, a bridge structure over or a tunnel under beneath the roadway. Conceptual sketches of both of these crossing options have been included in this section. The final Connectivity Study will identify the preferred crossing option.

#### 5.2.4 Summary Redevelopment Recommendations

1. Guide redevelopment of the Old Alabama Road corridor south of King's Market as a pedestrian-oriented office/mixed-use center.

- The existing super-blocks should be broken down into smaller blocks for redevelopment that have a pedestrian-oriented urban design character.
  - Parking facilities should be located behind buildings rather than adjacent to the street to support a pedestrian-oriented environment.
  - Incorporate the areas currently zoned OCMS and R-4 south of Holcomb Bridge Road (vacant property and Raintree Apartment Complex) into a single office/mixed-use zoning classification, which could be a revised version of the city's existing OCMS zoning.
  - Require the establishment of a distributed network of streets and improved connections and access to GA 400 through design guidelines and development regulations.
2. Promote and facilitate revitalization and redevelopment of C-3 zoned properties such as King's Market, Market Center at Holcomb Woods (1&2), and Holcomb Bridge Crossing Shopping Center areas.
    - Consider future mixed-use with residential and retail/services, including townhomes or condos at limited densities as an incentive to make redevelopment more economically viable.
    - Structured parking should be encouraged and hidden from view.
    - Pedestrian orientation should dominate the design for redeveloped centers.
    - Parks and greenspace should be established as central common areas.
    - A distributed network of streets should be established to form blocks and to improve local multi-modal transportation connectivity.
  3. Reduce traffic congestion at the intersections in the area and promote pedestrian activity in the node.
    - Maintain an acceptable roadway level of service (LOS) with redevelopment.
    - Reduce the amount of lane shifting required through the area's intersections.
    - Emphasize use of Market Boulevard for access south of Holcomb Bridge Road to and from the west.
    - Provide a safe pedestrian crossing of GA 400 and a crossing at Holcomb Woods Parkway.
    - Fill in existing gaps in the sidewalks throughout this area.
    - Convert existing wide concrete medians into landscaped medians where possible
  4. Provide a connection between the Chattahoochee River/Riverside Drive area recreation trails and amenities and the Big Creek multi-purpose trail.
    - Construct a bike/ped trail along the GA 400 right-of-way to connecting Riverside Drive to the Big Creek.
    - Provide a grade-separated crossing to connect trails on either side of HBR.

### Figure 20: GA 400/Old Alabama Character Sketches

Mixed-Use, Pedestrian -Oriented Environments and Infrastructure at a Community Business Center Scale.



### Figure 21: HBR/Old Alabama Intersection Streetscape Improvements



Current Intersection

Improved Intersection

Figure 22: Redevelopment in GA 400 / Old Alabama Node



Figure 23: Possible Bike/Ped Bridge over HBR @ GA 400





Figure 24: Possible Bike/Ped Bridge tunnel under HBR @ GA 400 (southern approach)



Figure 25: Possible Bike/Ped Bridge tunnel under HBR @ GA 400 (northern approach)

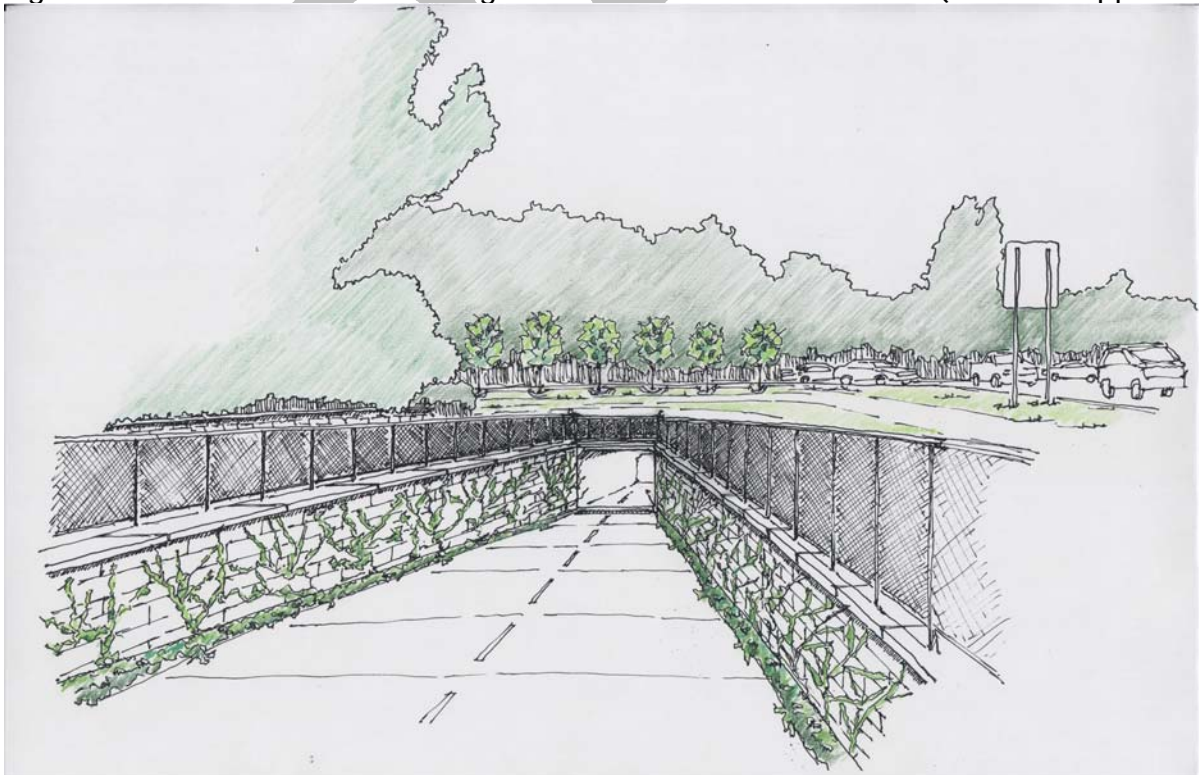
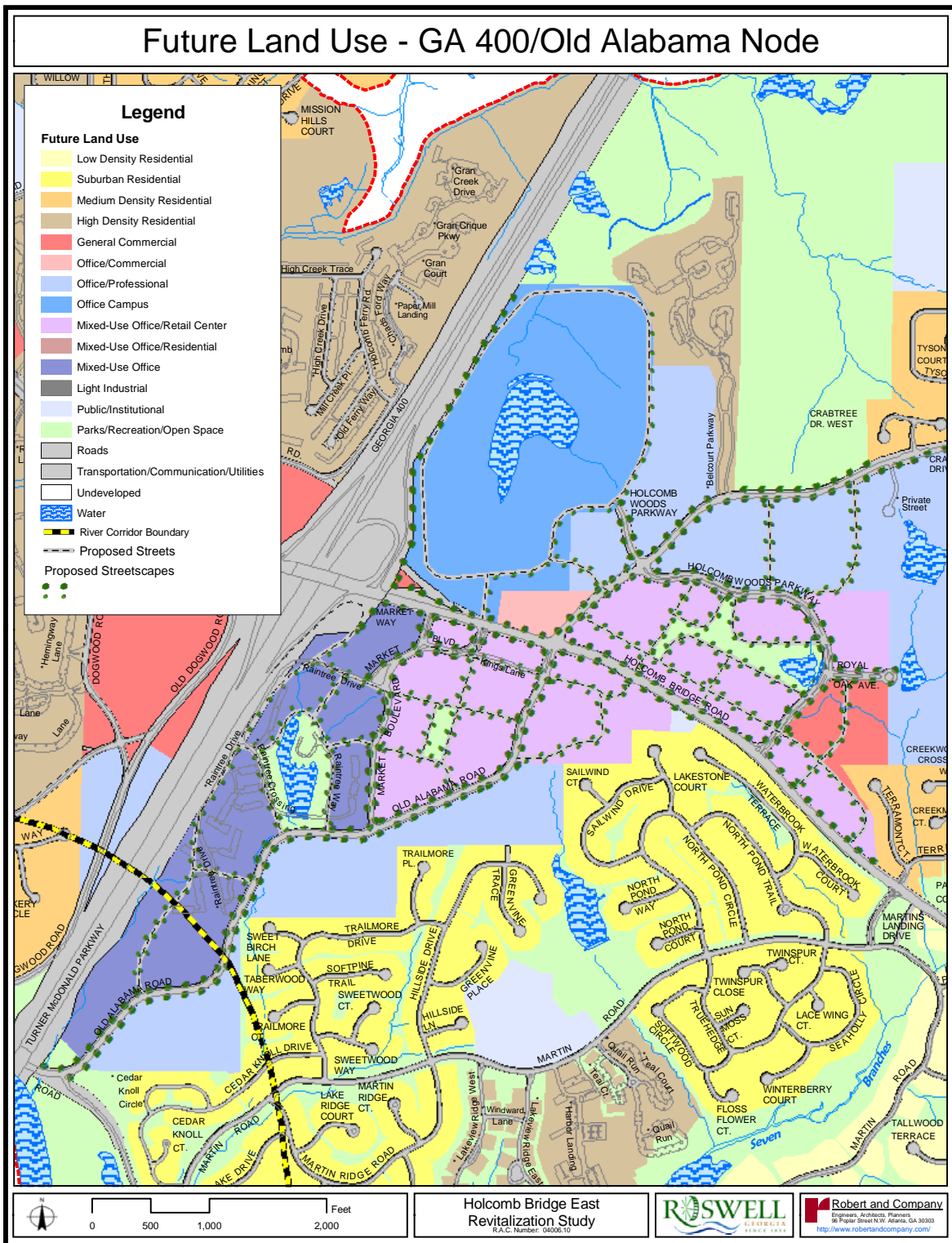


Figure 26: Future Land Use Concept for GA 400 / Old Alabama Node



## 5.3 The Centennial Node

### 5.3.1 Future Character and Goals

The Centennial Node area is located nearly five miles distance from the Historic Roswell civic facilities and functions and traffic congestion along that distance can easily cause the trip to be long and intimidating. Due to these factors and public comments compiled during the study process it was concluded that many residents of Roswell who live east of GA 400 have a desire for a community civic center that is closer than City Hall and the other civic facilities located in Historic Roswell. In the future, the Centennial Node should be more completely established as a community and institutional center for the East Holcomb Bridge area of Roswell. Additional civic and community facilities should be located in this node along with parks and trail system connections. Where possible, the character of new development in the area should emulate a town center environment with walkability, a mixture of uses and places for informal community gathering and celebration. The environment should be supportive of the needs of all ages, specifically as a focal point for families and children.

### 5.3.2 Revitalization Recommendations

Achieving the goal of establishing a true community center in the Centennial Node will require a combination of public investment in infrastructure and community facilities with infill development that is of appropriate use and design. There are no significant sites identified as being in need of redevelopment per se because the major developed areas are relatively new. However, the way that infill development occurs, and particularly how the undeveloped tracts along Holcomb Bridge Road are ultimately developed and/or preserved, is very important. Infill development near Holcomb Bridge Road should enhance the community center nature of the area. A mixture of land uses including residential, office and retail is appropriate. Development should follow design guidelines that promote pedestrian mobility and traditional neighborhood form.

Specific analysis of the approximately 49-acre site along the southern frontage of Holcomb Bridge Road suggests the potential for residential infill development while also creating a major greenway corridor parallel to Holcomb Bridge Road. A "conservation subdivision" approach should be used to plan this property for development of single-family residences. Additional multi-family housing (apartments), however, should not be encouraged here or elsewhere in the Centennial Node. Given the varied terrain of the site, a more detailed analysis of its development capacity should be undertaken to determine what density of development is possible.

The 28-acre site along the north frontage of Holcomb Bridge Road is much less impacted by environmental and topographic conditions. As a result, there are fewer design restrictions and more opportunities for a community center environment to be established. Current zoning allows for office development with some accessory retail commercial space. Office use is currently a missing component in the Centennial Node

area and the future development of professional offices will contribute to the general balance of uses that are supportive of a community center.

To further enhance the community center potential of the Centennial Node, substantial streetscape and bicycle/pedestrian improvements are needed to reinforce connections among existing (and future) community, residential, and retail uses. Specifically, bicycle/pedestrian and streetscape improvements are needed along Holcomb Bridge Road, Eves Road, Scott Boulevard and Fouts Road (connecting to East Roswell Park). Additionally, initial analysis identified the potential for a bike/ped crossing at Eves Road. The East Roswell Bicycle and Pedestrian Connectivity Study's preliminary recommendations show that this intersection is compatible with at-grade crossing in the short term, but due to projected traffic growth over time a grade-separation will be required to maintain safe crossing conditions. Topographically, the Eves Road intersection is one of the more suitable locations along HBR for a bridge over the roadway, which would provide an important linkage between the neighborhoods south of Holcomb Bridge Road (extending to Riverside Drive) and the schools on the north side, and the potential to establish a landmark in the community.

The Centennial Node is also the appropriate location along the Holcomb Bridge Road East corridor for development of a community center, library or similar public facility. This use is specifically recommended for the corner of Fouts Road and Holcomb Bridge Road, and linkages should be established to East Roswell Park and the schools.

### 5.3.3 Summary Recommendations

1. Establish the Centennial Activity Node as a community-oriented mixed-use activity center.
  - Maintain existing commercial centers with retail and services to serve neighborhood needs (grocery, etc.).
  - Promote the development of offices to complete the live-work-play equation within the Centennial Activity Node.
  - Further develop public facilities and institutions (schools, library, parks, etc.).
2. Improve connectivity, multi-modal transportation options and recreation amenities in the Centennial Activity Node.
  - Improve connections between neighborhoods and public facilities by installing multi-use trails connecting HBR to East Roswell Park along Fouts Road with an at-grade crossing at the Fouts and HBR intersection.
  - Enhance existing sidewalks with additional streetscape elements such as street trees and lighting and fill in any gaps in the node's existing sidewalk network.
  - Maintain a green buffer (with bicycle and pedestrian trails) along the south side of Holcomb Bridge Road between Martin's Landing entrance and Eves Road.
  - Develop and a multi use path connecting Eves Road / Martin's Landing with East Roswell Park with a bike/ped bridge over Holcomb Bridge Road near Eves Road.

- Add on-street parking on Scott Road to meet parking needs and shield pedestrians from traffic.

Figure 27: Centennial Node Character Sketches

Mixed-Use, Pedestrian-Oriented Community Center Environments.



Figure 28: Steetscaping of HBR in Centennial Node



HBR @ Centennial Node – Current Roadway



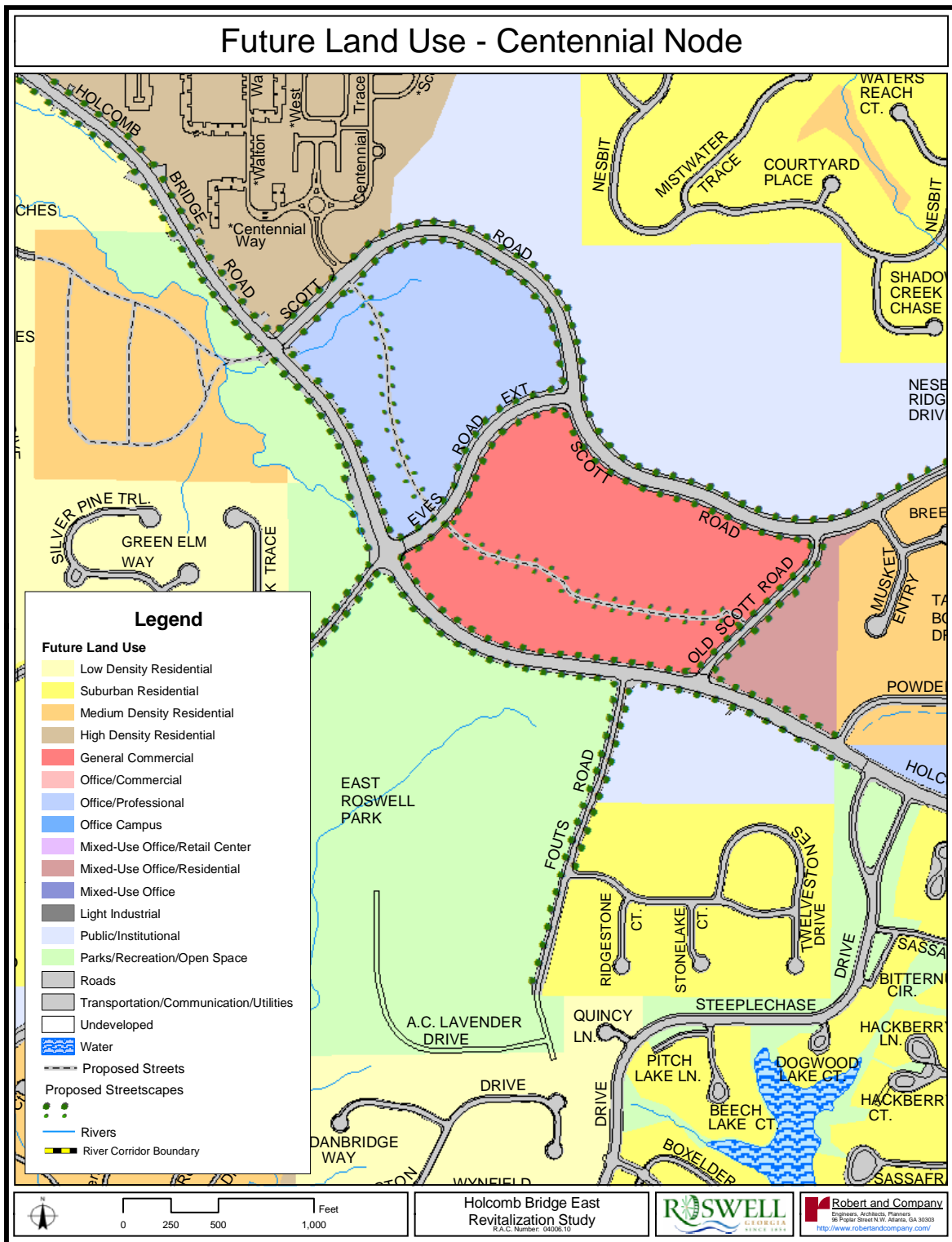
HBR @ Centennial Node – Recommended Streetscape

Figure 29: Potential Pedestrian Bridge Crossing HBR in the Centennial Node



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Figure 30: Future Land Use Concept for Centennial Node



## 5.4 Nesbit Ferry Node

### 5.4.1 Future Character and Goals

Located at the edge of the City of Roswell, the Nesbit Ferry Node should become an activity center for the surrounding population. Land uses within the node should include a diversity of residential types (including senior housing) and densities supported by up-scale retail and office development. The aesthetics, mobility and recreation opportunities in the area should be enhanced through improvements to the road rights-of-way, development of trails and enhancement of parks/public spaces.

### 5.4.2 Revitalization Recommendations

A redevelopment site combining the Tree Ridge property and surrounding parcels could generate sufficient revenues from various combinations of senior housing and medical office development to be economically viable for redevelopment. This site should be redeveloped so as to enhance the character of the corridor as a multi-generational community, offering rental housing for younger adults, single-family housing (both attached and detached) which appeals to families, and adding more options for seniors to remain in (or relocate to) the community through independent living units and assisted living units. Due to the existing density of multi-family development, this recommended redevelopment should not increase residential density in the area.

While new commercial development is not appropriate for this area, focus should be placed on long-term redevelopment and revitalization of existing centers. Commercial center improvement/redevelopment efforts should include recruiting an upscale chain grocery store (Whole Foods) as an anchor tenant in the area.

Capital improvements such as trails, streetscapes and parks/public spaces are also recommended, particularly trails or sidewalks that can serve to connect residential properties to the developing park at the Chattahoochee River. The Nesbit Ferry / Holcomb Bridge Road intersection has been identified within this study and by the East Roswell Bicycle and Pedestrian Connectivity Study as a recommended location for an at-grade bike/pedestrian crossing. Traffic engineering shows that this crossing can be achieved safely with minor intersection/signal modifications.

There is also public support for a community center in this area, though the location of a community center here would not be as central to east Roswell as would the Centennial Node location.

### 5.4.3 Summary Recommendations

1. Establish the Nesbit Ferry Node as a multigenerational activity center.
  - Promote the redevelopment of existing commercial land surrounding the intersection of Nesbit Ferry Road and Holcomb Bridge Road.
  - Use higher density housing (townhomes) and/or offices as a transitional use from retail commercial properties to single-family neighborhoods.



- Encourage the redevelopment of existing multi-family housing to senior housing with associated medical office facilities.
- Establish a multi-use path linkage from the Nesbit Ferry area to the Chattahoochee River at the Fulton County line.

Figure 31: Nesbit Ferry Node Character Sketches

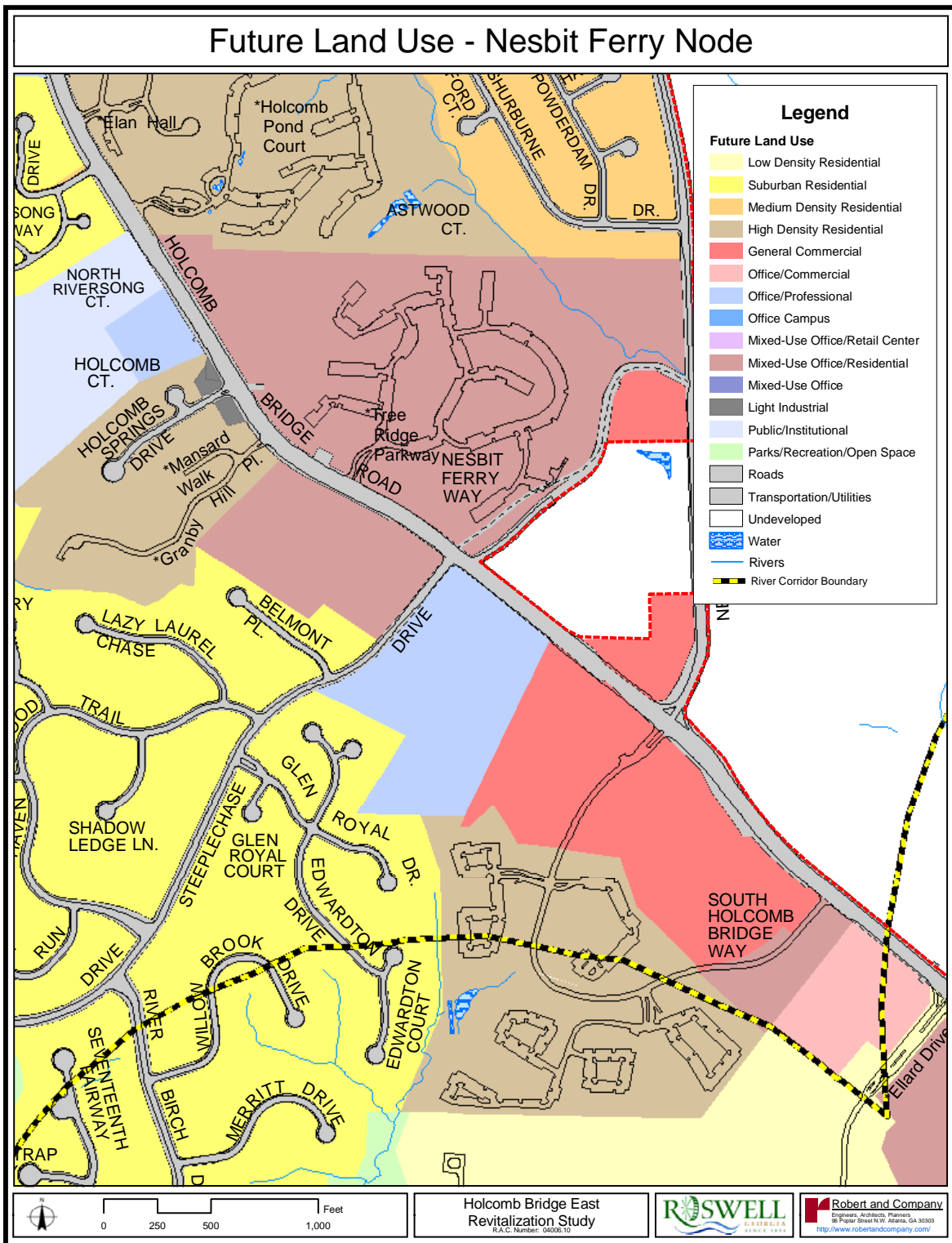
Pedestrian-Oriented Development with Senior Housing Community Architecture (building facades are near to streets preserving interior courtyard spaces).



Figure 32: Recommended Streetscape for Nesbitt Ferry Node



Figure 33: Future Land Use Concept for Nesbit Ferry Node



## 5.5 Inter-Nodal Areas

### 5.5.1 Future Character and Goals

In keeping with the overall goal of concentrating development into identified nodes rather than along the length of the corridor, the future character of the inter-nodal areas of the Holcomb Bridge Road east corridor should remain much as it is today: limited in development and traffic flow interruptions. The inter-nodal areas should have the feel and appearance of a "parkway" with landscaped buffers separating the roadway from trails and fence lines at the rear of residential properties. Due to the lack of curb cuts along these stretches of the corridor, there are many locations where the center turn lane of Holcomb Bridge Road is not necessary. In these locations, raised landscaped medians should be installed to enhance the parkway aesthetic of the roadway and promote efficient movement of traffic.

### 5.5.2 Revitalization Recommendations

The addition of raised landscaped medians to inter-nodal areas is the most significant improvement recommendation. Landscaped medians will have a dramatic effect on the parkway aesthetic of these sections of the corridor, and they will serve the function of continuing traffic flow as appropriate.

Recommended median placements along Holcomb Bridge Road are identified in Figures 34-39. These locations were considered in regards to traffic service and median break spacing compared to criteria in the Georgia Department of Transportation (GDOT) Regulations for Driveway and Encroachment Control (GDOT Driveway Manual), 2004. (see Appendix 7). Based on these factors recommendations for median design and placement along Holcomb Bridge Road were developed. The typical design recommended for median placement along Holcomb Bridge Road includes the following:

- Provide 11' travel lanes.
- Provide 11' turning lanes.
- Provide a 16' landscaped median between median break locations.
- Provide a 4' wide monolithic concrete median nose at intersections.
- Widen at intersections, as needed, to provide appropriate turning radius for u-turns from Holcomb Bridge Road.
- Provide median break spacing that is compliant with the GDOT Driveway manual.

Priority for investments in median construction should generally be given first to inter-nodal areas where minimal conflicts are likely. The corridor length between the GA 400/Old Alabama node and the Centennial Node should be first, followed by the corridor length between Centennial and Nesbit Ferry. Implementation of median additions within the defined nodes should be easier to accomplish once initial median projects between the nodes are successful. It will be generally appropriate to add median segments within the GA 400/Old Alabama node first, followed by the Centennial and Nesbit Ferry nodes.

Figure 34: Proposed Median Locations GA 400/Old Alabama Node

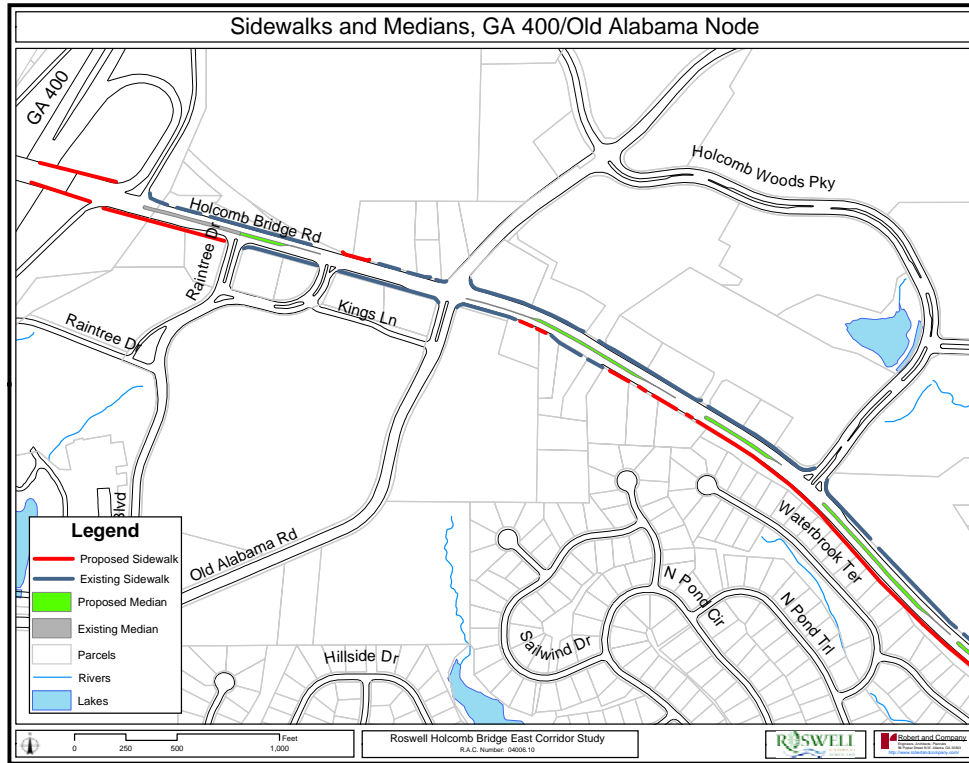


Figure 35: Proposed Median Locations: Old Alabama – Centennial Node

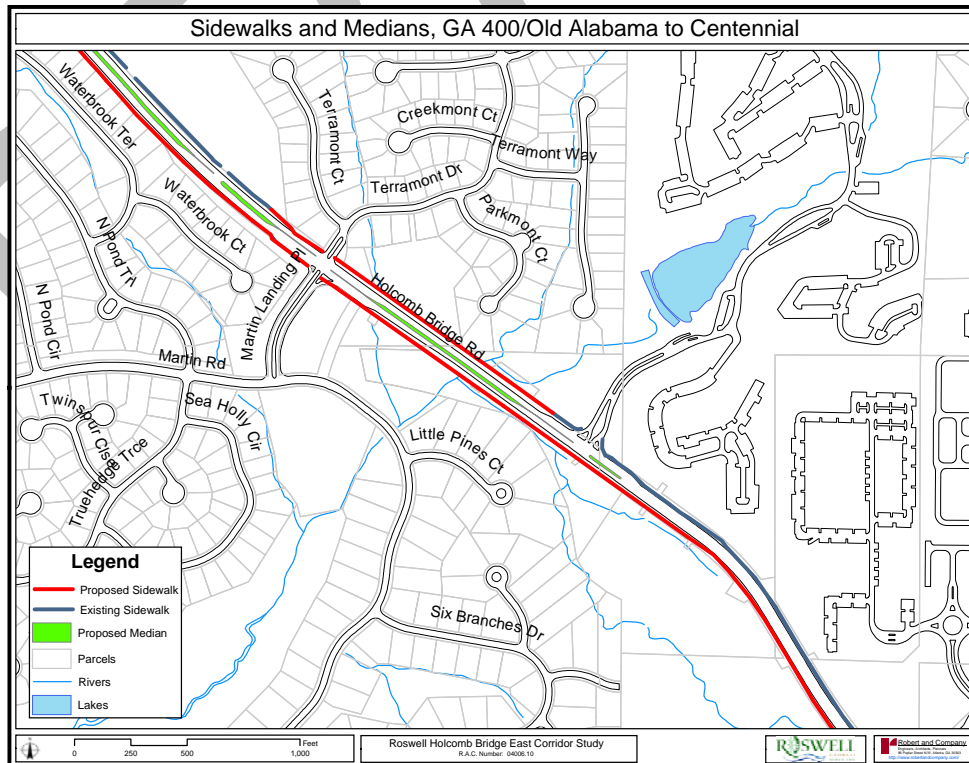


Figure 36: Proposed Median Locations Centennial Node

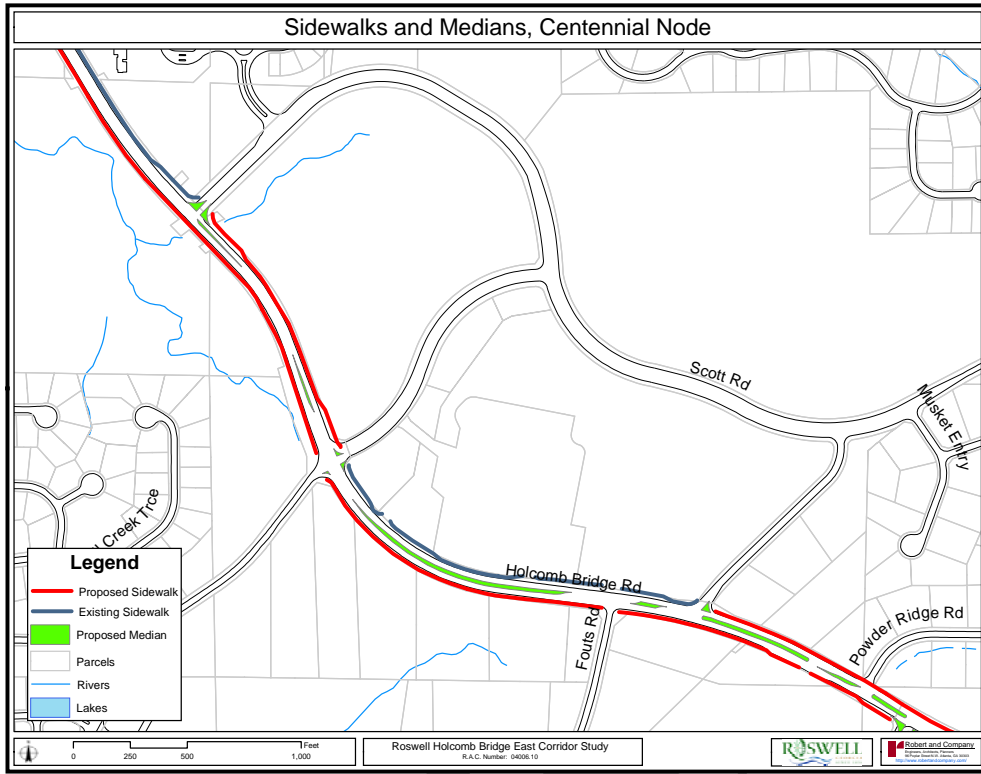


Figure 37: Proposed Median Locations: Centennial Node to Nesbit Ferry Node

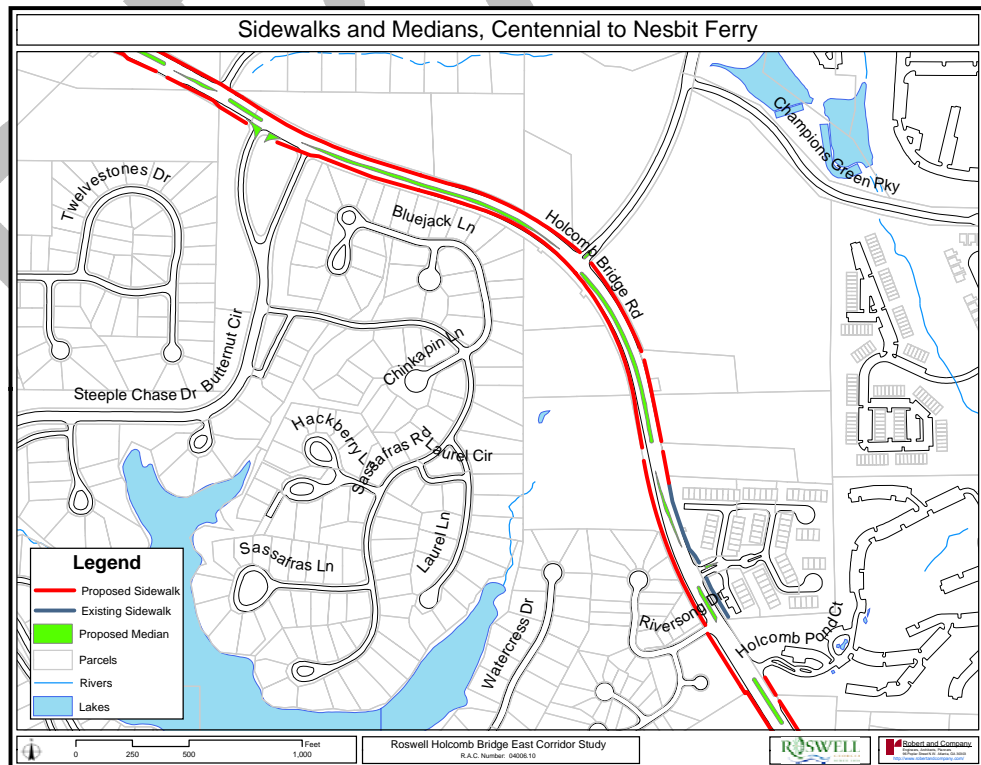


Figure 38: Proposed Median Locations Nesbit Ferry Node

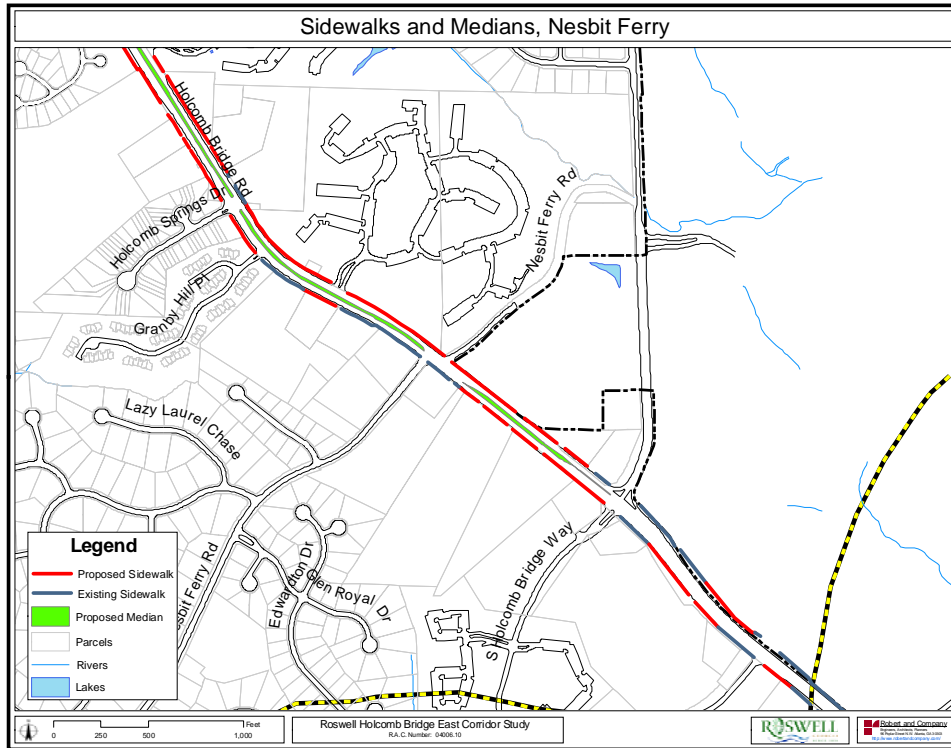
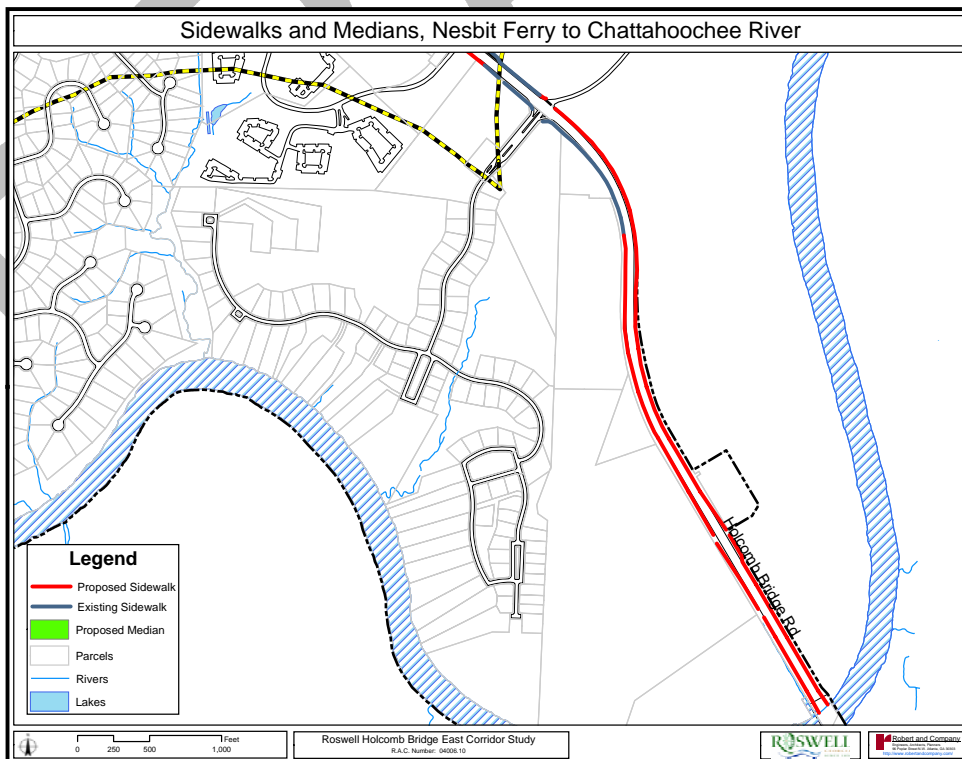


Figure 39: Proposed Median Locations Nesbit Ferry to Chattahoochee

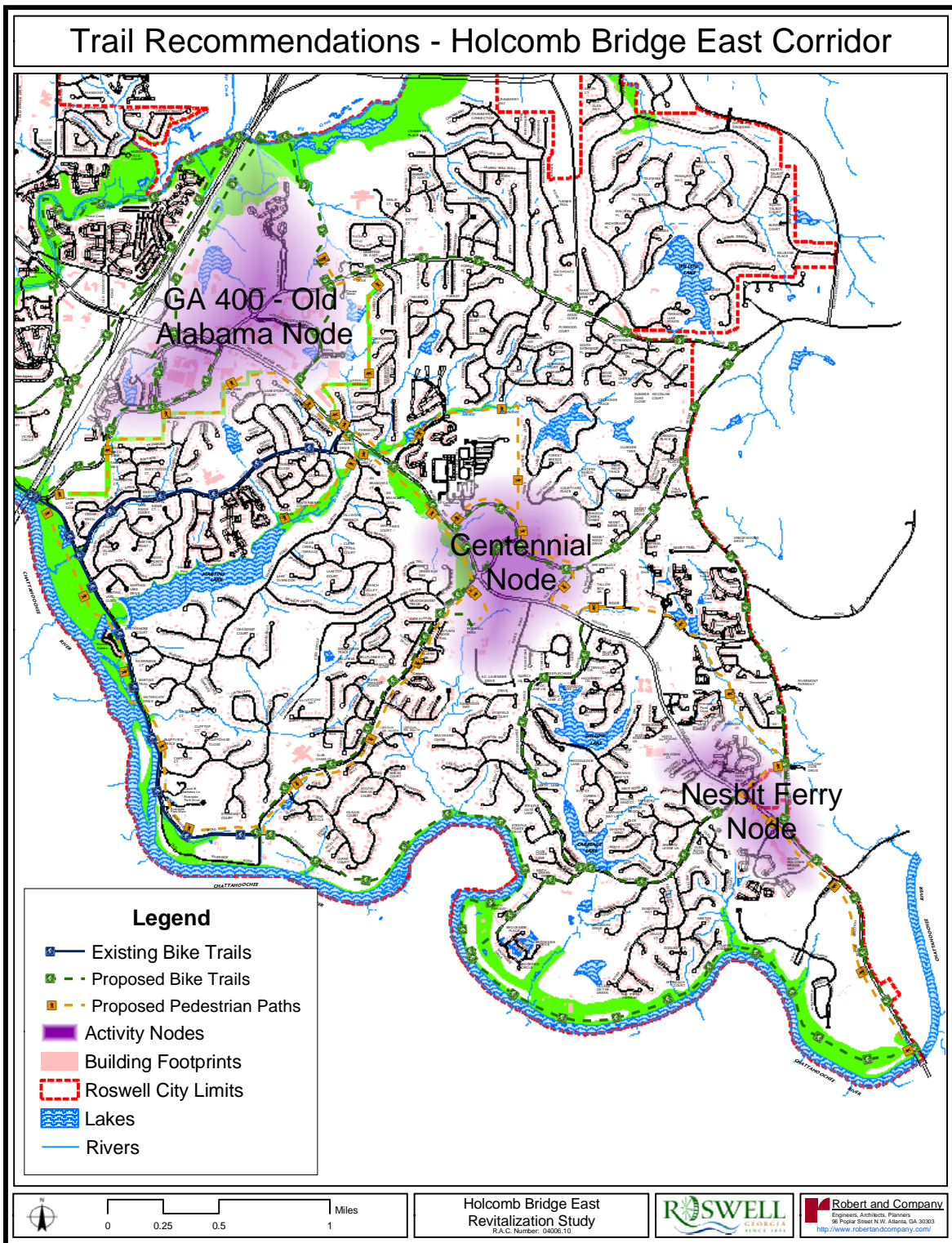


In addition to these proposed medians, there are numerous locations where there are gaps in the sidewalks connecting the nodes along the HBR corridor. These gaps should be filled in with the construction of new sidewalks. To enhance the “parkway” feel of the corridor sidewalk construction and enhancements should include streetscape elements such as street trees and green strips separating the sidewalk and roadway.

Along the Holcomb Bridge Road east corridor and extending into the communities to the north and south, a network of bicycle and pedestrian trails should be established to provide transportation and recreation opportunities (Figure 40). Such trails can help to define the “parkway” character of the corridor with coordinated landscape design features. Specifically, a greenway trail is recommended for the south side of Holcomb Bridge Road between the Martins Landing entrance and Eves Road. Due to the topographic conditions on this property, the trail will be required to be near the roadway in some locations and away from the roadway in others. A similar greenway trail should also be developed along the south side of Holcomb Bridge Road between the Nesbit Ferry Road area and the Chattahoochee River.

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Figure 40: Proposed Trail Locations





### Summary Recommendations

1. Improve the aesthetic appeal and improve/maintain traffic flow on Holcomb Bridge Road through the use of landscaped medians, landscaped sidewalks and roadside tree planting.
  - Landscaped medians should be installed where painted medians and concrete medians currently exist, particularly in areas between activity nodes.
  - Sidewalks should be connected and distanced from the roadway to the greatest extent possible.
  - Where present existing green buffers along the roadway should be maintained to enhance the parkway aesthetic.
2. Establish a broad network to link neighborhoods and parks with Activity Nodes via multi-use paths and greenways. Paths should be developed:
  - Along road rights-of-way (sidewalks and streetscape improvements, including bike lanes).
  - Along wetland/floodplain/creek corridors (riparian buffers).
  - Along major buffers between differing land uses.
  - Connecting to, within and through public parks and green space corridors.

Figure 41: Inter-Nodal Character Sketches

Parkway Road Environments with Landscaped Medians and Tree-lined Roadway Edges.



Figure 42: Proposed Typical Landscaped Median for Holcomb Bridge Road



# **CHAPTER 6**

## *IMPLEMENTATION*

### 6.1 Implementation Strategies and Recommendations

- 6.1.1 Implementation Committee
- 6.1.2 Design Guidelines
- 6.1.3 Merchant's Association
- 6.1.4 High Impact Short Term Improvements
- 6.1.5 Community Improvement District
- 6.1.6 Tax Allocation District / Tax Increment Financing
- 6.1.7 Traffic Coordination

### 6.2 Architectural and Landscape Design Recommendations

- 6.2.1 Design Recommendations: The Street
- 6.2.2 Design Recommendations: Site Planning and Buildings
- 6.2.3 Building/Architectural Design

### 6.3 Recommended Regulatory Changes

- 6.3.1 Recommended Comprehensive Plan Changes
- 6.3.2 Necessary Zoning Changes

### 6.4 Implementation Plan

## 6.1 Implementation Strategies and Recommendations

The realization of the Vision for the Holcomb Bridge Road East Corridor created through this study process is dependent on the success of a number of capital improvement and redevelopment projects. However, the completion of these projects is contingent upon the strong organizational mechanisms to provide leadership for the visionary projects. Also required will be a number of city-initiated regulatory changes. Key strategies for implementing the recommended changes along the corridor are as follows:

### 6.1.1 Implementation Committee

The success of the implementation efforts associated with this revitalization plan are dependant upon the commitment and consensus of the corridor's property owners, residents, merchants and, perhaps above all the city's elected officials. It is recommended that the steering committee members continue to promote projects in the study area. The city's Economic Development Manager should work with this committee in coordination with other Community Development department staff.

### 6.1.2 Design Guidelines

Guidelines for the design of buildings and the environment in the Holcomb Bridge Road East area will assist with short, middle and long-term revitalization goals. Currently, the RiverBanks Campus District Overlay Design Guidelines are in place for the study area. While these existing guidelines address a number of design issues that are of concern in the study area, modifications and additions are recommended to deal with redevelopment planning issues. Section 6.2 provides an analysis of areas where the RiverBanks guidelines can be combined with guidelines from the Midtown Roswell area and some new design requirements to create a set of design criteria that will promote the vision for the corridor proposed in this document.

### 6.1.3 Merchant's Association

Existing area businesses could consider forming a merchant's association to help retain current businesses and attract new business to East Holcomb Bridge Road retail centers. To address issues in the specific commercial nodes along the corridor, the merchant's association could have committees for each node. Initiatives that could be undertaken by this organization in the near term include: 1) developing new opportunities for advertising the shops and businesses along the corridor such as a local shopping and dining guide and 2) partnering with the city's Economic Development staff to recruit new neighborhood serving businesses to the corridor and fill existing vacancies.

### 6.1.4 High Impact Short Term Improvements

A series of high visibility improvements accomplished in the near term will enhance the aesthetics of the corridor, help gather public support for revitalization efforts and have a positive effect on the economic viability of the area. The inter-nodal median

improvements recommended throughout the Corridor are the initial steps in this program.

#### 6.1.5 Community Improvement District

To help finance improvements and revitalization projects in the corridor area, local businesses could consider promoting the establishment of a Community Improvement District (CID) for the corridor. A CID is a financing mechanism for infrastructure projects and programs. CIDs are becoming popular around the state as a method of improving areas and attracting redevelopment. Property owners should initiate the creation of a CID. A majority of eligible property owners that represent at least 75 percent of the tax value of the eligible properties in the proposed district must agree to the additional millage rate. Only commercial and industrial properties are eligible to be included in a CID. The city must also pass a resolution allowing the district.

#### 6.1.6 Tax Allocation District / Tax Increment Financing

Another financial tool available to the City for the revitalization of the Holcomb Bridge Road East Corridor is the formation of a Tax Allocation District (TAD). A TAD can be created that allows the City to take advantage of the increase in property tax revenue due to implementation of the redevelopment projects for some of the large retail centers and older apartment complexes described above. The property tax base would be “frozen” at a specified time and the increase in the tax base as a result of the redevelopment could be used to provide funding in the form of TAD bonds that can pay for capital improvements in the district, such as:

- Sidewalks/Pedestrian bridges
- Multi Use Trails
- Community Facilities
- Roads
- Parks

#### 6.1.7 Traffic Coordination

Traffic congestion created by commuters traveling through the Holcomb Bridge Road East Corridor on their way to and from work has a negative impact on quality of life in the area. While this study outlines a number of roadway and intersection improvement projects to be undertaken to alleviate congestion and limit negative impacts, the City should continue to work in partnership with Fulton County and GDOT to fully address this issue. The City must have a meaningful role in the development of all transportation plans affecting the greater Holcomb Bridge Road area.

## 6.2 Architectural and Landscape Design Recommendations

The RiverBanks Campus District Overlay Design Guidelines were approved in 1999 to serve as design guidelines for an area including the entirety of the study area for this report. On the east side of GA 400, this area is approximate to the GA 400/Old Alabama Node that has been identified in this study. The existing RiverBanks Campus District guidelines address many design issues that are of concern in the study area, and have a positive impact on new development. Improvements to these guidelines are needed, however, to address revitalization planning issues.

Design guidelines have also been established for the Midtown Roswell area subsequent to the completion of the Midtown Roswell Redevelopment Plan. Adopted in 2003, these design guidelines offer specific guidance to development in support of redevelopment and revitalization concepts. Based on a review of these adopted guidelines, there are many provisions that are being applied to the Midtown Roswell area that should also be applied to the Holcomb Bridge East area in support of similar redevelopment and revitalization goals. The following sections include a more specific indication of ways that the Parkway Village, Midtown Roswell, and RiverBanks Campus Design Guidelines can be adapted and applied to the Holcomb Bridge East area in support of redevelopment strategies recommended in this study.

### 6.2.1 Design Recommendations: The Street

The essence of design guidelines is a definition of the appropriate design and development of sites in relation to the public street. This includes significant attention to the area of transition from street to building, which includes private property as well as public right-of-way. Streets differ in size, capacity and function, so design guidelines should also differ according to the nature of the street. However, there are some basic design principles that should be encouraged throughout the Holcomb Bridge Road East study area in support of redevelopment, revitalization and the community's expressed vision for the community environment.

#### *Street Types*

Classifying streets by type is a common practice in planning and usually relates primarily to issues of traffic volume capacity. Differentiating types of streets is also important from a design standpoint. The names that we use today for streets are derived from design character in many cases: boulevard, avenue, lane. Design guidelines should establish a vocabulary of street types that have specific associated design characteristics. For the Holcomb Bridge Road study area, these street types should include the following at minimum: Parkway, Boulevard, Avenue, Street and Alley. Holcomb Bridge Road, for example, should have the design character of a Boulevard within the activity node areas and the character of a Parkway in between activity nodes. The characterization of street types provides a basis for more specific design guidelines for public rights-of-way.

### *Sidewalks/Streetscape*

Both the RiverBanks and Midtown Roswell design guidelines address appropriate design of sidewalks and streetscape elements, but there are some differences in detail. The RiverBanks guidelines consider a narrow grass strip between sidewalk and curb to be marginally appropriate while the Midtown Roswell guidelines consider this inappropriate due to maintenance difficulty. Overall, however, both guidelines support wide sidewalks, street trees and other streetscape element such as pedestrian lighting. For the Holcomb Bridge East area, streetscape guidelines that are appropriate to the different street types should be established to enhance the existing guidelines.

### *Curb Cuts, Driveways and Medians*

The Midtown Roswell design guidelines include standards for design of curb cuts and driveways that promote consolidation of driveways and narrowing of driveway aprons. The RiverBanks guidelines do not similarly address curb cuts and driveways. These guidelines are appropriate for the Holcomb Bridge Road area, for Holcomb Bridge Road and other major roads such as Old Alabama Road. Additionally, design guidelines for medians should be incorporated into the recommendations for street designs such as Boulevards and Parkways.

Figure 3:  
Proposed Driveway Apron

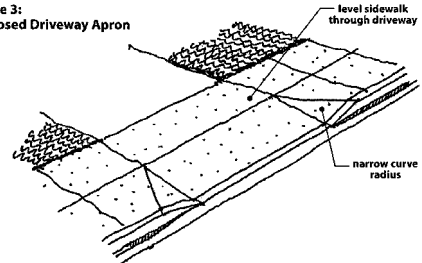
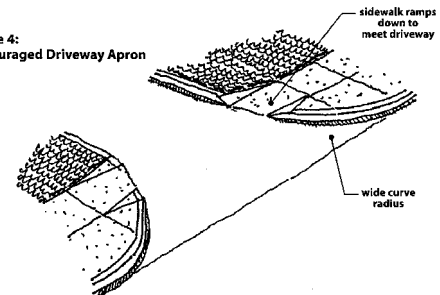


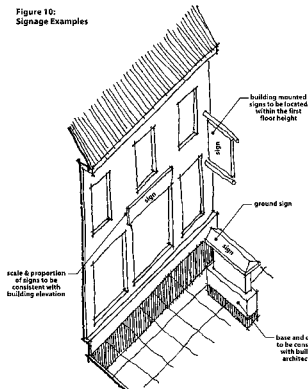
Figure 4:  
Discouraged Driveway Apron



### *Signage and Screening*

Both RiverBanks and Midtown Roswell guidelines address design of signage and screening. Design requirements include orienting signage to pedestrians by locating at the ground level and coordinating the details of signs with the architectural details of the buildings that they serve. Due to the width of the Holcomb Bridge Road corridor and the typical traffic speeds, it is appropriate for signage to be presented at the ground level (monument signs) and also at higher levels (building mounted signage) as indicated in the RiverBanks guidelines (Criteria #5, Marginal Rating depiction).

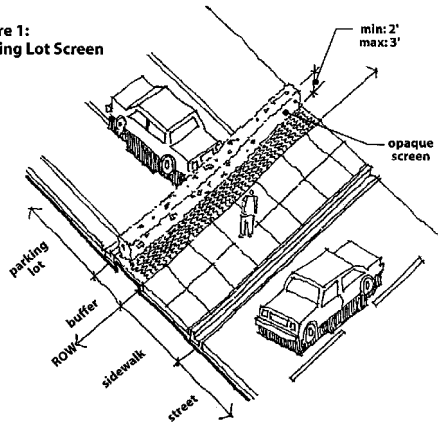
Figure 10:  
Signage Examples



RiverBanks guidelines include standards for screening parking lots from view from the roadway with landscaping and also for screening blank walls (building or site) with landscaping. The Midtown Roswell guidelines include requirements for screening parking lots as well as loading docks, refuse areas, service and utility areas, and mechanical equipment with

fencing and landscaping. This requirement should be applied to the Holcomb Bridge Road East area as well.

Figure 1:  
Parking Lot Screen



### 6.2.2 Design Recommendations: Site Planning and Buildings

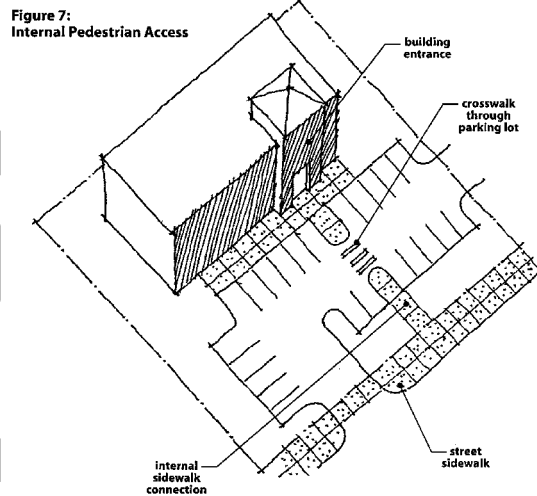
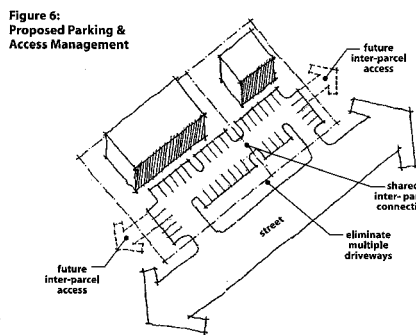
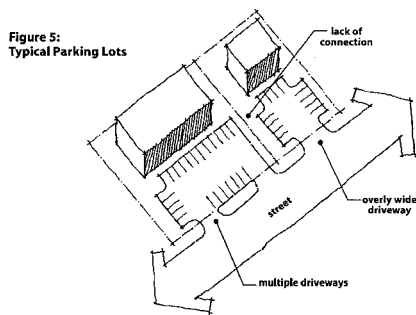
The layout and design of development sites has a profound impact on the overall environment. In general, the conventional approach to site design in recent decades, the time period when most commercial development along Holcomb Bridge Road occurred, favored automobile accessibility over pedestrian and aesthetic considerations. Today, the expectations have changed and a return is being made to promoting pedestrian-oriented environments where automobile access is accommodated but hidden to the extent possible. Standards for design of parking, building design and landscaping are key components of site design.

#### *Parking*

Facilities (or lots) for parking cars have become the dominant use of land in highway commercial environments. For decades, zoning codes have supported the notion that the design of commercial developments must first accommodate the automobile in the most efficient manner, putting pedestrian and aesthetic considerations as secondary. This has resulted in the typical arrangement of a large store or strip of stores separated from the street by vast parking lots. The parking lots are typically hostile to any pedestrian movement from street/sidewalk to building, and they are increasingly recognized as the primary factor in aesthetic blight.

New thinking about the proper design of parking lots in commercial development has taken a cue from design models of the past, where buildings were brought near to the sidewalk and parking was located behind the buildings, as is commonly seen in "Main Street" downtowns. Though some retailers may complain that "hidden" parking discourages customers due to the perception that there is no parking available, this is increasingly disputed by successful "neo-traditional" developments that have been designed without highly visible parking lots.

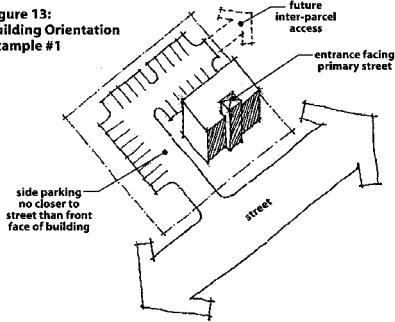
The Midtown Roswell Design Guidelines include effective guidelines for parking lot design following two scenarios. To address existing surface parking lots that are adjacent to the street/sidewalk, guidelines are provided for screening, landscape islands, internal pedestrian access and inter-parcel access design. A requirement for screening parking lots from the public right-of-way is also included in the RiverBanks guidelines, but the Midtown Roswell guidelines provide more specific and appropriate guidelines. Guidelines for landscape islands, internal pedestrian access and inter-parcel access design are not included in the RiverBanks guidelines, though a "Signature Element" is encouraged which is a concept for a low wall at property edges. It would be appropriate for the Midtown Roswell guidelines for screening, landscape islands, internal pedestrian access and inter-parcel access to be adapted and applied to Holcomb Bridge East.



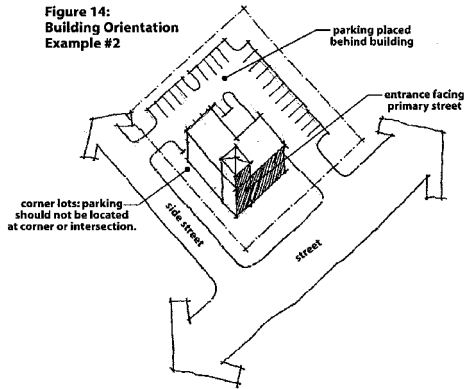
For instances of new site and building design, the Midtown Roswell guidelines include requirements for the placement and orientation of buildings and parking facilities. Buildings must be placed and oriented towards primary street frontage and parking lots located behind buildings or to the rear of the parcel. In cases where parking decks are constructed as part of a development, the parking deck structure must also be located behind buildings or, alternatively, incorporate architectural space and elements when they front internal roads. These parking lot design standards are not included in the RiverBanks guidelines, however they should be adapted and applied to the Holcomb Bridge East area as they are especially salient to commercial shopping center redevelopment needs.



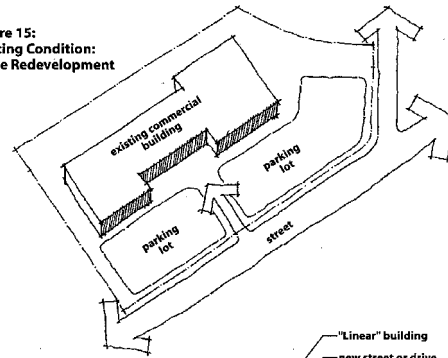
**Figure 13:  
Building Orientation  
Example #1**



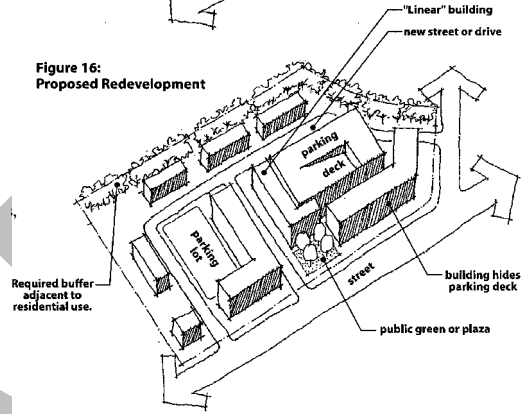
**Figure 14:  
Building Orientation  
Example #2**



**Figure 15:  
Existing Condition:  
Large Redevelopment  
Site**



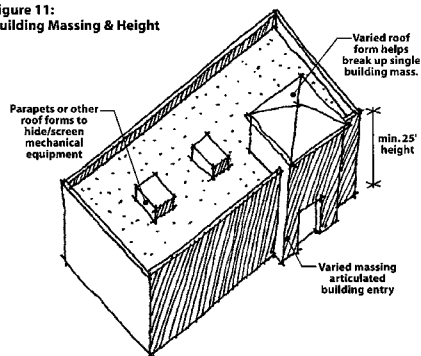
**Figure 16:  
Proposed Redevelopment**



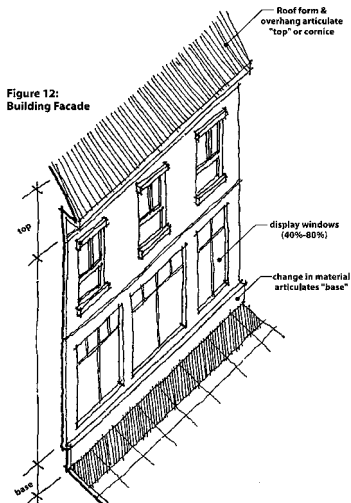
### 6.2.3 Building/Architectural Design

The RiverBanks and Midtown Roswell design guidelines address appropriate building and architectural design requirements in a variety of ways. RiverBanks guidelines specify appropriate visual patterns on building facades and require contextual sensitivity in the design of new buildings. The Midtown Roswell guidelines are more specific as they address building mass and height, rooflines and roof materials, building materials and color, building façade design and, very importantly, façade setback and build-to requirements. A combination of the requirements from both design guidelines should be used for the Holcomb Bridge Road East study area. Some adjustments to the building setback/build-to requirements will be needed, particularly in regard to Holcomb Bridge Road due to traffic volume and speed considerations.

**Figure 11:  
Building Massing & Height**



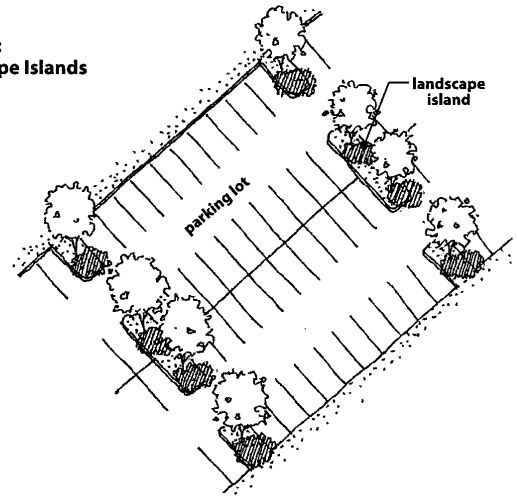
**Figure 12:  
Building Facade**



#### 6.2.4 Landscape Design and Materials

The RiverBanks Campus District design guidelines strongly encourage lush landscaping in the environment, particularly at the edges of development between roadways and parking lots. Also included is a signature plant list for the district, essentially a list of suggested trees, shrubs and groundcover plants to be used in landscaping. Though it is not intended to be a complete or required list of acceptable materials for landscaping, the signature plant list may be too limited, including only six species of trees. For example, Crape Myrtle trees are not included on the list, though they are very hearty and attractive in this type of environment. The White Oak tree is included on the list, but the Willow Oak, also a native species, would be preferable in this environment. It is recommended that the signature plan list be revised and expanded and that landscape design guidelines also include specific requirements for landscape islands in surface parking lots as illustrated in the Midtown Roswell design guidelines.

Figure 2:  
Landscape Islands



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### 6.3 Recommended Regulatory Changes

In order to effectively implement the Holcomb Bridge Road East Revitalization Plan, some amendments and additions to the City of Roswell's Comprehensive Plan and Zoning Ordinances are necessary.

#### 6.3.1 Recommended Comprehensive Plan Changes

The city is in the process of updating its Comprehensive Plan. It is recommended that the City adopt policies that support the goals of this study as part of that process. The city's current Comprehensive Plan includes a range of policies that support this study's goals including supporting the city's stable neighborhoods, encouraging redevelopment of economically deteriorating areas, redevelopment of commercial uses and nodal development techniques. The table below lists changes to the "Guiding Principles" of the Future Land Use Element and applicable policy statements from other elements of the plan that would bring the document into full support of this study's goals and objectives. The city should also add the list of implementation items contained in Section 6.4 to the city's Short Term Work Program, which will be updated along with the Comprehensive Plan.

Guiding Principle or Policy Element & Pg. #	Issue	Recommendation
Future Land Use – Use Classifications pages 425 – 429	The current use classifications do not allow for a mixed-use development.	Mixed use, depending on how it's defined, may be a viable option in the study area and warrants further discussion by Mayor and Council.
Encourage redevelopment of economically deteriorating areas. Future Land Use Pg. 432	Policy statement refers to "suburban-style" commercial developments such as those located on the corridor but does not specifically state the type of redevelopment that should be encouraged for these deteriorating centers.	Incorporate this study and its revitalization recommendations by reference.
Emphasize redevelopment over expansion of commercial uses into new and unforeseen areas Future Land Use Pg. 432	This policy statement could be construed to encourage redevelopment of strip centers in the corridor to allow for greater commercial square footage which is counter to this study's commitment to reducing the commercial square footage in the corridor as it is thought to be excessive compared to sq. footage / resident ratios in other communities.	Restate policy to specifically encourage the reduction in commercial square footage through the redevelopment of declining commercial centers.

Guiding Principle or Policy Element & Pg. #	Issue	Recommendation
Protect the capacity of major thoroughfares through nodal development techniques. Future Land Use Pg. 433	The corresponding policy statement encourages residential subdivisions with reverse frontage lots or multi-family as appropriate uses between development nodes and allows for office development when residential is not feasible. However, due to the challenging topography of the corridor there are many areas of Holcomb Bridge Road frontage that are not appropriate for residential or office development.	The policy statement should be expanded to encourage the conservation of undeveloped roadway frontage for environmental and recreational purposes.
Encourage or require the planting of street trees in subdivisions. Natural Resources, Pg. 210	This plan includes numerous recommendations for landscaping and the planting of trees along the corridor and within its nodes.	Expand this policy or add a policy to encourage or require the planting of in appropriate locations within activity centers and along pedestrian corridors.
City-Wide Design Criteria / RiverBanks Design Guidelines Urban Design Pg. 306	The City-Wide criteria listed here are based on the RiverBanks guidelines which have been recommended for revisions and adoption for the entire study area. These criteria do not adequately address the pedestrian environment.	These criteria should be amended to include requirements for street trees and/or pedestrian lighting in activity nodes.
Community Facilities, Library, Pg. 500	Study identifies the desire for a new Fulton County branch library in East Roswell.	A policy statement could be added to recommend the city continue to encourage and work with the County on the development of a branch library in East Roswell.
Transportation: Bicycle and Pedestrian Plan pages 668-686	This study identifies a number of pedestrian improvements needed in the short term (2006 – 2011 period). While most of these are already listed in the city's Bike/Ped plan the project prioritization does not coincide in some instances.	Amend the Bike/Ped plan to move the pedestrian improvements included in this study's 5 year implementation plan into the 2006 – 2011 implementation period of the Bike/Ped plan.

### 6.3.2 Recommended Zoning Changes

#### *Mixed-Use Districts:*

The city's existing OCMS zoning classification should be re-examined to accommodate potential mixed-use projects with the inclusion of a residential component. An appropriate mix to achieve the goal of encouraging office development with appropriate accessory residential and retail uses would be 60% office, 20% residential (not to exceed a height of 45-feet and 8 units per acre), 10% retail and 10% open space (in addition to the minimum open space requirement currently in the OCMS district). To

minimize land use conflicts, the type of development outlined here is only appropriate in existing OCMS zones within the study area that are not adjacent to properties zoned for industrial uses. Additionally, any development permit must be accompanied by a detailed examination of the design requirements that would be appropriate to ensure quality development compatible with the surrounding environment in these targeted areas.

*Design Guidelines:*

To ensure high quality development and redevelopment throughout the corridor especially in the identified activity nodes, the existing RiverBanks design guidelines should be amended as advised in Section 6.2.

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## 6.4 Implementation Program

The following tables list transportation and other local projects and actions that should be implemented over the next five years or in the longer term to implement this plan. Please note that these conceptual cost estimates have been made carefully, but RAC assumes no liability for omissions or errors.

### Transportation Implementation Projects

Project ID	Phase	Project Name	Engineering / Consultant Year	Engineering Costs	Construction Year	Construction Costs	Total Project Costs	Local Match	Responsible Party	Potential Funding Sources	ROW Required <sup>7</sup>
<b>Streetscape Improvements and Median Installation</b>											
S1	5-year	GA 400/Old Alabama Node - Streetscape <sup>1</sup>	2007	\$49,050	2008-09	\$327,100	\$376,150	\$75,230	City of Roswell	City, TE, LCI	No
S2	5-year	GA 400/Old Alabama Node - Landscaped median installation	2007	\$135,800	2008-09	\$905,400	\$1,041,200	\$208,240	City of Roswell	City, LCI, GDOT Safety	No
S3	5-year	GA 400/Old Alabama to Centennial Link - Streetscape <sup>1</sup>	2006	\$56,550	2007	\$377,150	\$433,700	\$86,740	City of Roswell	City, TE, LCI	No
S4	5-year	GA 400/Old Alabama to Centennial Link - Median installation <sup>2</sup>	2006	\$160,700	2007	\$1,071,300	\$1,232,000	\$246,400	City of Roswell	City, LCI, GDOT Safety	No
S5	5-year	Centennial Node - Streetscape <sup>1</sup>	2008	\$160,150	2009	\$1,067,750	\$1,227,900	\$245,580	City of Roswell	City, TE, LCI	No
S6	5-year	Centennial Node - Landscaped median installation <sup>3</sup>	2008	\$150,300	2009	\$1,001,850	\$1,152,150	\$230,430	City of Roswell	City, LCI, GDOT Safety	No
S7	5-year	Centennial to Nesbit Ferry Link - Streetscape <sup>1</sup>	2007	\$158,100	2008	\$1,053,850	\$1,211,950	\$242,390	City of Roswell	City, TE, LCI	No
S8	5-year	Centennial to Nesbit Ferry Link - Landscaped median installation <sup>3</sup>	2007	\$145,050	2008	\$966,950	\$1,112,000	\$222,400	City of Roswell	City, LCI, GDOT Safety	No
S9	5-year	Nesbit Ferry Node - Streetscape <sup>1</sup>	2009	\$145,950	2010	\$972,850	\$1,118,800	\$223,760	City of Roswell	City, TE, LCI	No
S10	5-year	Nesbit Ferry Node - Landscaped median installation	2009	\$138,150	2010	\$921,050	\$1,059,200	\$211,840	City of Roswell	City, LCI, GDOT Safety	No
S11	5-year	Nesbit Ferry to Chattahoochee Link - Streetscape <sup>4</sup>	2008	\$97,650	2009	\$651,000	\$748,650	\$149,730	City of Roswell	City, TE, LCI	No
<b>Roadway / Operations Improvements</b>											
R1	5-year	Provide supplemental overhead signage on Holcomb Bridge Road in advance of Old Alabama Road <sup>5</sup>	2006	\$25,200	2006	\$168,000	\$193,200	\$38,640	Georgia Department of Transportation	GDOT Safety	Yes
R2	5-year	Modify SR 400 SB off-ramp to provide dual left turn lanes	n/a	Planned GDOT Maintenance Project					Georgia Department of Transportation	Planned GDOT Maintenance Project	No

Project ID	Phase	Project Name	Engineering / Consultant Year	Engineering Costs	Construction Year	Construction Costs	Total Project Costs	Local Match	Responsible Party	Potential Funding Sources	ROW Required <sup>7</sup>
R3	5-year	Extend existing WB right turn lane at SR 400 NB on ramp to Market Boulevard	2007	\$23,650	2008-09	\$157,500	\$181,150	\$36,230	Georgia Department of Transportation	STP, CMAQ	Yes
R4	5-year	Construct second free-flow lane from SR 400 NB off-ramp to EB Holcomb Bridge Road to connect directly to Market Way	2007	\$64,700	2008-09	\$431,200	\$495,900	\$99,180	Georgia Department of Transportation	STP, GDOT Safety, CMAQ	Yes
R5	5-year	Restripe Scott Road from Holcomb Bridge Road to Old Scott Road to provide two through lanes with parallel parking (includes pavement topping).	2008	\$23,150	2009	\$154,350	\$177,500	\$35,500	City of Roswell	City, LCI	No
R6	Long - Range	Modify Market Boulevard and Market Way south of Holcomb Bridge Road to provide connection to the roadway network in conjunction with redevelopment.	TBD	Coordinate Funding and Implementation with Future Redevelopment Effort				City of Roswell	City, LCI	Yes	
R7	Long - Range	Modify Holcomb Bridge Road from Market Boulevard through Old Alabama Road: <ul style="list-style-type: none"> <li>Continue third EB through lane through Old Alabama Road intersection to remove lane drop at EB dual left turn.</li> <li>Add a second SB right turn lane to provide a dual right turn with protected signal phasing.</li> <li>Provide an additional WB through lane from east of Old Alabama Road to SR 400 (connect to WB right turn lane extended in five-year implementation plan).</li> </ul>	TBD	Long-Range Cost to be Determined as Project is Developed in the Future				City of Roswell	STP, GDOT Safety, CMAQ, LCI	Yes	
<b>Pedestrian / Bike Trail Improvements</b>											
P1	5-year	Implement pedestrian accommodations along the north side of Holcomb Bridge Road overpass of SR 400 <sup>6</sup>	2008	\$21,700	2008-09	\$144,600	\$166,300	\$33,260	Georgia Department of Transportation	GDOT Safety, LCI	No
P2	5-year	Install pedestrian/bike multiuse trail grade separation of Holcomb Bridge Road between SR 400 and Market Boulevard. Provide connection to regional trail network.	2008	TBD	2008-09	TBD	TBD	TBD	City of Roswell	City, LCI	Yes

Project ID	Phase	Project Name	Engineering / Consultant Year	Engineering Costs	Construction Year	Construction Costs	Total Project Costs	Local Match	Responsible Party	Potential Funding Sources	ROW Required <sup>7</sup>
P3	5-year	Install pedestrian/bike accommodations along Fouts Road (including intersection with Holcomb Bridge Road and phasing modifications to provide protected pedestrian/bike trail crossing movement)	2009	\$62,950	2009	\$419,600	\$482,550	\$96,510	City of Roswell	City, LCI	Yes
P4	Long - Range	Include crossing at Nesbit Ferry Road intersection with Holcomb Bridge Road (phasing modifications to provide protected pedestrian/bike trail crossing movement)	TBD	Long-Range Cost to be Determined as Project is Developed in the Future				City of Roswell	City, LCI	Yes	
P5	Long - Range	Install pedestrian/bike multiuse trail grade separation of Holcomb Bridge Road in the vicinity of Eves Road. Provide connection to regional trail network.	TBD	Long-Range Cost to be Determined as Project is Developed in the Future				City of Roswell	City, LCI	Yes	

<sup>1</sup> Includes 5-foot sidewalks, landscaping, lighting, and benches

<sup>2</sup> Includes landscaped median west of Martins Landing Drive and 4-foot concrete median east of Martins Landing Drive

<sup>3</sup> Includes channelization islands at select intersections along the corridor

<sup>4</sup> Includes 5-foot sidewalks on north side of Holcomb Bridge Road, multi-use trail on south side of Holcomb Bridge Road, landscaping, lighting, and benches

<sup>5</sup> Two locations including signage and supporting structure

<sup>6</sup> Sidewalk and fencing on SR 400 bridge and pedestrian crossings at ramp intersections

<sup>7</sup> Cost estimates include construction and planning/engineering costs - right of way costs are not included.

TE = Transportation Enhancement

STP = Surface Transportation Program

LCI = LCI Implementation Funds

CMAQ = Congestion Mitigation and Air Quality

GDOT Safety = GDOT Safety and Operations Improvement

City = City of Roswell



### Housing Initiatives

Description/Action	Cost	Year	Responsible Party	Funding Source
Revise zoning ordinance to include limited residential development in OCMS districts located with the study area, which are not adjacent to industrial districts.	N/A	2006	City	City

### Other Local Initiatives

Description/Action	Cost	Year	Responsible Party	Funding Source
Continue to assist area property managers in efforts to attract quality tenants to fill vacant space in existing commercial centers in the study area.	N/A	On going	Economic Development Manager	N/A
Continue to support development of additional community facilities, such as a Fulton County branch library in the Centennial Node.	TBD	2006	City	City, County

## 6.5 Population and Employment Estimates

The following table includes population and housing estimates based on the continuation of current conditions as defined by the build-out analysis of the city contained in the Roswell Comprehensive Plan 2020 and estimates that include the Comprehensive Plan build-out modified by the implementation of recommendations in this plan.

Table 8: Population and Housing Estimates

	2000	2005	2010	2015	2020	2025
<b><i>CURRENT CONDITIONS</i></b>						
Population	28,336	30,828	34,178	35,665	36,615	37,590
Housing Units	12,069	12,971	13,671	14,266	14,646	15,036
Estimated New Housing Units from Study		25	208			
Cumulative New Units Added		25	233	233	233	233
<b><i>CONDITIONS WITH STUDY IMPLEMENTATION</i></b>						
Population		30,828	34,240	36,248	37,198	38,173
Housing Units		12,971	13,696	14,499	14,879	15,269

Table 9 provides an estimate of the employment in the study area under two scenarios; the continuation of current employment trends, and the employment which would be located in the corridor if the development plan recommended by this study is implemented.

Table 9: Employment Estimates

	2000	2005	2010	2015	2020	2025
<b><i>CURRENT CONDITIONS</i></b>						
Employment in Study Area	10,397	11,428	12,617	13,505	14,233	14,945
Estimated New Office Development in Study Area (sq. ft.)			250,000	250,000	250,000	250,000
Estimated New Retail Development in Study Area – no net increase in retail square footage is anticipated						
<b><i>CONDITIONS WITH STUDY IMPLEMENTATION</i></b>						
Base Employment		11,428	11,428	12,261	13,094	13,927
Additional Office Employment (1 employee / 300 sq. ft.)			833	833	833	833
Additional Retail Employment (1 employee / 600 sq. ft.)						
Total Estimated Employment		11,428	12,261	13,094	13,927	14,760

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## Appendices

1. Shopper Survey and Summary of Shopper Survey Results
2. Summary of Visual Preference Survey Results
3. Summary of Comments for Kick-Off Public Meeting
4. Summary of Design Charette Comments
5. List of Stakeholders Interviewed
6. Retail Market Analysis Tables
7. Median Location Recommendations

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Appendix 1  
Shopper Survey & Summary of Shopper Survey Results

***City of Roswell—Holcomb Bridge East Revitalization Study  
Retail Shopping Survey***

The City of Roswell is interested in learning about the retail experience of those who live and shop in the Holcomb Bridge East Corridor (The portion of Roswell east of GA 400). **If you are a resident or business in this area,** please take a few minutes and complete the survey and return it to us either by fax or mail at the address indicated at the end of the form. Please, only one survey per household. Look for a summary of the results of the survey on the City of Roswell’s website in June.

1. Where do you live?
  - a. Roswell \_\_\_\_\_
  - b. Other place (please name)\_\_\_\_\_
  
2. If you live in the City of Roswell, on what street or in subdivision do you live?
  - a. Street:\_\_\_\_\_
  - b. Subdivision/Complex\_\_\_\_\_
  
3. How long have you lived at this location? (please circle) 1 2 3 4 5 6 7 8+ years
  
4. **Where** do you typically **shop for the following items** (store name or shopping center name-Location i.e. Home Depot, Roswell; Nordstrom’s, Perimeter Center)
  - a. Groceries\_\_\_\_\_
  - b. Pharmacy\_\_\_\_\_
  - c. Dry Cleaning\_\_\_\_\_
  - d. Fast Food\_\_\_\_\_
  - e. Restaurant\_\_\_\_\_
  - f. Home improvement/hardware\_\_\_\_\_
  - g. Kids toys/games\_\_\_\_\_
  - h. Books, CDs DVDs\_\_\_\_\_
  - i. Clothing—kids\_\_\_\_\_
  - j. Clothing—adults\_\_\_\_\_
  - k. Home furnishings\_\_\_\_\_
  - l. Electronics\_\_\_\_\_

5. What **percentage** of your shopping do you do...
- a. On the way **to** work \_\_\_\_\_ %
  - b. On the way **from** work \_\_\_\_\_ %
  - c. During lunch or breaks at work \_\_\_\_\_ %
  - d. From home \_\_\_\_\_ %
  - e. Internet or catalogs \_\_\_\_\_ %
- 100%

6. When is your favorite **time to shop during the week**?
- a. Weekends \_\_\_\_\_
  - b. Weekdays \_\_\_\_\_

7. What is your favorite **time to shop during the day**?
- a. early morning (8-11a.m.) \_\_\_\_\_
  - b. lunch time (11a.m. to 1 p.m.) \_\_\_\_\_
  - c. afternoon (1p.m.-5p.m.) \_\_\_\_\_
  - d. rush hour (5p.m.-7p.m.) \_\_\_\_\_
  - e. evening (7p.m.-10p.m.) \_\_\_\_\_
  - f. late night (10p.m. or later) \_\_\_\_\_

8. What kinds of retail/retailers would you like to see come **to Holcomb Bridge Road**?

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9. What do you **like** about **Holcomb Bridge Road** as a place to shop?

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10. What do you **dislike** about **Holcomb Bridge Road** as a place to shop?

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11. Where is your favorite place to shop **in Atlanta**? Why?

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10. How many **people** in your household? \_\_\_\_\_

11. How many **children** (1-17 years of age) in your household? \_\_\_\_\_

12. How many household members are **employed outside the home**? \_\_\_\_\_

13 Do you **own or rent** your home? (circle answer) Own Rent

14 Which category represents **your household income**?

- a. Less than \$25,000 \_\_\_\_\_
- b. \$25,000-49,000 \_\_\_\_\_
- c. \$50,000-\$99,000 \_\_\_\_\_
- d. \$100,000-\$149,000 \_\_\_\_\_
- e. \$150,000 or more \_\_\_\_\_

Thank you for completing this shopper preference survey. This information will be used to help the City of Roswell plan for improvements to the Holcomb Bridge Road Corridor.

Please either mail or fax your completed surveys **no later than May 16, 2004** to:

**Susan Ricker**  
**City of Roswell – Economic Development**  
**38 Hill Street, Suite G-30**  
**Roswell, GA 30075**  
**Fax: (770) 641-3741**

## **SUMMARY OF FINDINGS: HOLCOMB BRIDGE CORRIDOR SHOPPERS SURVEY**

### **ABOUT THE SURVEY:**

Questionnaires were distributed during the period April 15, 2004-May 21, 2004 at public meetings and on the City of Roswell web site; 187 were returned and analyzed.

### **THE RESPONDENTS:**

- 98% live in the City of Roswell.
- Nearly all live in single-family homes in one of the major subdivisions in the Holcomb Bridge Road Corridor. Only one respondent lived in a townhome; none lived in an apartment.
- 99% are homeowners.
- Nearly two-thirds (64%) have lived in their current home for six years or more; 13% have lived in their current home for 1-2 years.
- 44% of responding households report income of \$150,000 or more per year; less than 4% report income below \$50,000; 52% have income between \$50,000 and \$150,000.
- The high incomes reflect the fact that 43% of the households have two or more residents working outside the home; however, an equal number of homes report only one person working outside the home.
- 45%% of the households are composed of two people; 9% are one-person households; 35% are three or four-person households; and 11% have five or more people.
- Well over half the responding households (57%) include no children; 14% have one child; 20% have two children; 9% report three or more children.

### **WHERE THEY SHOP:**

- Convenience goods are purchased overwhelmingly in Corridor stores:
  - o *Groceries*: 62% shop along Holcomb Bridge Corridor; 24% buy groceries in the Alpharetta/North Point area; 12% shop in Roswell outside the Corridor (predominantly at Publix).
  - o *Pharmacy*: 78% shop in stores within the Corridor; 10% buy from other pharmacies in Roswell; 11% buy in Alpharetta/North Point.



- o *Dry cleaning:* 83% use cleaners located within the Corridor; 7% use other cleaners in Roswell; 4% go to Alpharetta/North Point; and 6% go elsewhere, presumably on the way to/from work.
- o *Fast food:* 83% pick up fast food within the Corridor, although 11% go to Alpharetta/North Point and 4% patronize other fast food establishments elsewhere in Roswell.
- Soft and hard goods are generally purchased outside the Corridor with one major exception...
  - o *Home improvements/Hardware:* 84% patronize the Home Depot located within the Corridor; 11% go to other stores in Roswell.
  - o *Kids toys/Games:* More than half (56%) the respondents shop for this in Alpharetta/North Point although 33% do buy these items within the Corridor.
  - o *Books, CDs, DVDs:* 82% purchase these items in Alpharetta/Roswell; 4% shop within the Corridor.
  - o *Kids clothing:* 57% shop for this item in Alpharetta/North Point although 27% buy within the Corridor.
  - o *Adults clothing:* 54% of the respondents go to Alpharetta/North Point while 21% shop within the Corridor; 10% shop the Perimeter area for this item.
  - o *Home furnishings:* 60% shop in Alpharetta/North Point for these items; 8% shop in the Perimeter area; 7% shop elsewhere in Roswell; 6% shop in the Corridor; and 19% look in other parts of Atlanta.
  - o *Electronics:* 80% go to Alpharetta/North Point for this category; 7% shop within the Corridor; 2% go elsewhere in Roswell.

**WHEN THEY SHOP:**

- Weekdays are favored by 71% of the respondents; weekends by 29%.
- Early morning (8-11 a.m.) and afternoon (1-5 p.m.) are each favored by 33% of the households; lunch time (11a.m.-1 p.m.) is favored by 20%; evenings (7-10 p.m.) are favored by 8%; rush hour (5-7 p.m.) by 5%; no one liked to shop late at night.
- 11% of the households do at least half their shopping on the Internet or via catalogues.

- The reported number of respondents who shop on the way *to* work is negligible.
- Half the respondents do some shopping (but less than 50%) on the way *from* work.

**PERCEIVED ADVANTAGES OF SHOPPING IN THE CORRIDOR:**

- 82% like the convenience and that it is close to home.
- 6% report that they like the stores in the Corridor.
- Other reasons include: easy access and the variety offered.

**PERCEIVED DISADVANTAGES OF SHOPPING WITHIN THE CORRIDOR:**

- 47% cited heavy traffic.
- 10% listed lack of store variety.
- 9%, no upscale shops.
- 4% not enough grocery choices.
- 28% cited other disadvantages including: difficulty of discerning what stores are in the centers; empty stores; perception of danger; too many open parking lots; poor design; centers are old and outdated; new centers are ugly; undesirable businesses are moving in.

**WHAT RETAILERS WOULD BE DESIRABLE:**

- 27% reported more grocery choice, citing specifically a desire to shop at Publix and Whole Foods.
- 21% want more restaurant/entertainment.
- 10% would like a discount/big box retailer.
- 8% want a Target.
- 5% listed a bookstore.
- 29% wanted a range of other retailers including: specialty stores such as boutiques, craft shops, The Container Store, Restoration Hardware, gift stores; community activity centers such as an aquatic center, YMCA; movie theaters.

## **FAVORITE PLACE TO SHOP IN ATLANTA:**

- 35% North Point
- 24% Perimeter Center
- 11% Lenox/Phipps
- 30% Other, including Avenues at East Cobb, The Forum on Peachtree Parkway
- Reasons: Controlled access, pedestrian friendly

## **KEY OBSERVATIONS:**

- Respondents in the study area are affluent residents of single-family housing in two-three person households with significant discretionary income.
- They do extensive shopping in the Holcomb Bridge Corridor for groceries and convenience items, hardware, services, and restaurants.
- They are more likely to go outside the Corridor for major hard and soft goods purchases.
- They would like to see a broader range of retailers in the Corridor, including
  - o Restaurants
  - o Discount/ big box stores
  - o Grocery choices

Appendix 2  
Summary of Visual Preference Survey Results

## **Robert and Company**

*Engineers, Architects, Planners, Landscape Architects  
96 Poplar Street, N. W.  
Atlanta, Georgia 30303  
(404) 577-4000 FAX: (404) 577-7119  
www.robertandcompany.com*

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### **Memorandum**

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**To: East Holcomb Bridge Road Revitalization Study Steering Committee**  
**From: Robert and Company**  
**Re: Community Preference Survey Results Summary**  
**Date: June 17, 2004**

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The Community Preference Survey was conducted as part of the Public Meeting on April 27, 2003. Robert and Company prepared the survey as a series of 100 images of the built environment, including images from the East Holcomb Bridge Road area and other locations. Survey participants were asked to use provided score sheets to rate each image on a scale from -3 to +3 based solely on individual preference.

#### **Overall Analysis of Preferences and Dislikes**

From the raw score results of 82 preference survey responses, the top ten and bottom ten images have been identified. Following are assumptions derived from review of the top and bottom ten images.

#### **Preferences – Overall**

- Traditional architectural design, brick construction, etc.
- Designed landscaping with shade trees, good maintenance, etc.
- Wide sidewalks and plaza-type spaces.
- Traditional Neighborhood Development (TND).
- A focus on the river and related recreational amenities.
- Formal and informal parks and open spaces.

#### **Dislikes – Overall**

- Strip commercial style development.
- Large set backs and parking lots in front of buildings with no landscaping, trees, etc.
- “Modern” buildings, i.e. non-traditional architecture with non-traditional materials.
- Garden style apartments and townhouse developments that lack landscaping and trees.

#### **Categorical Analysis of Preferences and Dislikes**

The 100 images included in the preference survey were categorized into four sections of 25 images. The first section was themed as “Housing”, the second as “Commercial”, the third as “General Development” and the fourth as “Parks/Open Space”. Survey respondents were not informed of the intentional categorization of images.

The most consistent positive responses in the survey were within the Parks and Open Space category. There negative raw scores in this category were exclusively related to images presenting wide roadways and incidents of potential auto/pedestrian conflict.

The following analysis by category has considered the top and bottom five images in each category.

#### **Preferences and Dislikes – Housing**

- General character preference is for heavily landscaped residential development.
- Preference for housing in a mixed-use setting (2 to 3 stories, residential over retail).
- General character dislike is of monotonous designs and single use high-density communities.

#### **Preferences and Dislikes – Commercial**

- Development type preference for semi-urban, mixed-use development with a moderate level of density.
- Development type dislike of strip commercial and/or stand alone big box commercial development.

#### **Preferences and Dislikes – General Development**

- Preferences are for “Main Street” style development, wide sidewalks, landscaped parking lots and street trees.
- Dislike of non-traditional, modern, or unusual architecture.

#### **Preferences and Dislikes – Parks and Open Spaces**

- Preference for formal and informal open spaces.
- Preference for semi-urban/downtown types of spaces, detailed hardscapes with pavers, designed well-kept landscaping, shade trees, water features (fountains, etc.), paths and sidewalks separated from traffic.
- Focus on the river and related recreational activities.
- Dislike of wide roadways where pedestrians are not buffered from automobile travel lanes.
- Dislike for open space lacking trees.

Appendix 3  
Summary of Comments from Kick-Off Public Meeting

## **Robert and Company**

*Engineers, Architects, Planners, Landscape Architects*  
96 Poplar Street, N. W.  
Atlanta, Georgia 30303  
(404) 577-4000 FAX: (404) 577-7119  
[www.robertandcompany.com](http://www.robertandcompany.com)

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### **MEMORANDUM**

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**Re: Holcomb Bridge East Revitalization Study**  
**To: City of Roswell**  
**Cc: Ken Bleakly, Bleakly Advisory Group**  
**Date: May 6, 2004**

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The following is a summary of comments received from the public during the April 27, 2004 Public Meeting held at the East Roswell Recreation Center.

1. East Holcomb Bridge Road (EHBR) does not have an identity like historic Roswell; there is no reason for people to stop. The area's "core" needs to be defined. Increased pedestrian access will not solve these issues.
2. EHBR cannot support additional retail square footage. More homes should be built along the corridor, instead of additional retail or office space.
3. Zoning for retail use is too easy to obtain.
4. Old A&P (or other vacant "big-box" retail) could be converted into a year-round competition style aquatic center; this would create a destination. This use makes a lot of sense for the area and would be well supported by the citizens.
5. A vacant retail center could be reused as a center for activities related to health care and wellness.
6. The intersection of HBR and Old Alabama Road needs improvements to connect retail centers and allow pedestrians to walk between them.
7. A pedestrian bridge should be constructed across EHBR for students crossing to the south side of the road from the elementary and high school near Centennial.
8. EHBR needs grocery stores; specifically the community would like a Whole Foods and a Publix.
9. Community is concerned that traffic will be worse when the vacant retail stores fill up.
10. The east side of Roswell needs a library; citizens would like the revitalization study to recommend steps they take to help secure a new library for the area.



11. EHBR needs a YMCA. Community facilities like this can become a real focal point for the community. Creation of a “central place” for East Roswell might best be tied to the development of community-serving recreational facilities.
12. There needs to be better interconnectivity of the trails in the area.
13. The revitalization study should take into consideration MARTA’s long term goals for transit stations at the HBR/400 and Mansell Road intersections.
14. People are drawn to newer, prettier retail centers; this is why owners don’t renovate older centers. The North Point retail center has sucked all the retail away from EHBR.
15. The Parkway Village retail center has lots of demand and interest, why can’t EHBR command this? Parkway Village is a good model with calm traffic and a nice environment where it’s easy to shop.
16. EHBR needs to be made more attractive and peaceful.
17. Citizens want to know what can be done to stop Wal-Mart and other big-box retailers from holding onto abandoned retail spaces. The City should provide tax incentives to promote redevelopment and to help shopping center owners release retailers that are holding onto vacant spaces from their leases. The city council has limited control over what retailers do; their actions are subject to private agreements between landlords and retailers and are not public decisions.
18. Some citizens expressed the opinion that the City of Roswell needs to be more accommodating of local retailers, and that current regulations are too strict, for example the limit on Home Depot’s pine straw sales has led to some complaints. The City is running the risk of losing remaining retailers if it continues to hold to such strict regulations. The retail that remains on EHBR is primarily neighborhood serving retail; national retailers have gone elsewhere because of the perception that it is hard to do business with the City of Roswell.
19. EHBR needs to “regentrify” existing office and retail centers and promote mixed-use.
20. The current configuration of retail centers positioned below grade with large set backs has led to their decline, these designs are not congruent with current standards and images of success.
21. The revitalization study must recommend for how to redevelop vacant nodes and how to develop remaining vacant parcels. The study should identify other cities that have development comparable to what is recommended for the revitalization of EHBR.
22. The City has to give retailers a reason to come back; the study should suggest incentives that could be provided to retailers to locate on EHBR. Why can’t the City provide incentives to property owners to improve the retail center facades and to bring in tenants the community wants.

23. Citizens questioned whether the City has the funds to purchase additional right of way to accomplish streetscape improvements that would add to the visual appeal and driver and pedestrian comfort along EHBR.
24. Citizens had varied reactions when asked how they feel about the idea of mixed-use redevelopment. Some said it's a scary idea this is often talked about too generally. Some stated they do not think it is appropriate for EHBR. There was a general perception that there is already mixed use in the EHBR corridor area. Some citizens noted that mixed use is the way "real" cities like Charleston and Seattle work and that this type of development helps to keep the areas vital and vibrant. Citizens are concerned about where mixed-use developments would go, some feel the corridor is already at it max density. It was noted that larger, vacant or declining retail centers could be adapted into mixed-use developments or city greens, however all their traffic cannot be funneled onto EHBR and Old Alabama. It was also stated that the new Ellard development is not mixed use or new urbanism, it's a strip center in front of a high end subdivision.
25. Citizens had mixed reactions to the idea of additional town homes or multi-family housing development along the EHBR corridor. Many feel that the corridor already has enough of this type of development. Other citizens recognize that there is a demand for cluster homes and other higher density type developments. Developments for citizens over 55 could be appropriate for the EHBR corridor. As with any type of development landscaping and trees make a big difference in how well the development fits in with the surroundings and is accepted.
26. EHBR needs bike paths; a citizen suggested that these should be separated from the roadways with large set backs and earth berms. Paths should be divided with defined spaces for cyclists and pedestrians and a minimum of 8 feet wide.
27. Smart lights should be installed along EHBR to help reduce congestion and keep traffic moving better.
28. EHBR has to carry through traffic and local traffic.
29. Quality of life issues must be made compatible with realistic business issues.

Appendix 4  
Design Charette Comments Summary

# **Robert and Company**

*Engineers, Architects, Planners, Landscape Architects*  
96 Poplar Street, N. W.  
Atlanta, Georgia 30303  
(404) 577-4000 FAX: (404) 577-7119  
[www.robertandcompany.com](http://www.robertandcompany.com)

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## **Memorandum**

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**To: East Holcomb Bridge Road Revitalization Study Steering Committee**  
**From: Robert and Company**  
**Re: Summary of Comments from Design Charette**  
**Date: September 23, 2004**

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A Design Charette was conducted as part of the Public Meeting on July 13, 2004. Participants held discussions in groups focused on three identified nodes of the Holcomb Bridge East Corridor: 400/Old Alabama Node, Centennial Node, Nesbit Ferry Node. The following is a summary of the key comments for each node provided by the various charette teams.

### **400/Old Alabama Area:**

- Develop a senior day care and assisted living facility in the area that includes neighborhood commercial uses and is not “institutional” but has a village feel
- Streetscape improvements are needed to encourage bike and pedestrian activity between HBR / Old Alabama node and new park
- Traffic engineering is needed to improve traffic flow, improvements are needed along HBR, Old Alabama, and Holcomb Woods Pkwy
- Higher density residential (condos/town homes) is appropriate for the area between Kings Market center and GA 400
- Redevelop Kings Market into a Mixed Use – Transit Oriented Center with a MARTA connection (park and ride now, rail later), offices and hotels with conference spaces
- Redevelopment of retail centers on north side of HBR (Market Centers) should include underground parking to raise development to street level A community center should be developed in the area as part of a mixed-use redevelopment of an existing retail center
- The existing green buffer between Old Alabama Road and the Martins Landing subdivision should be maintained

### **Centennial Area**

- This area needs substantial streetscaping and bike/ped improvements to reinforce connections between existing community, residential, and retail uses (improvements needed along HBR, Eves, Scott Blvd, and Fouts)
- A pedestrian / bike bridge is needed at Eves Rd and HBR
- Sidewalks are needed along Fouts to provide a connection with East Roswell Park

- A community center / library / or other public use is recommended for the corner of Fouts Road and HBR
- Additional apartment development is not appropriate for the area, however limited townhomes with a large minimum square footage may be appropriate
- Large-lot (1 acre) single family residential development is appropriate for the vacant land on the south side of HBR from Martin's Landing to Eves
- Some mixed use development, specifically office/retail and live/work townhomes, are appropriate for this area

#### **Nesbit Ferry Area**

- Additional town home and office development is appropriate for vacant parcels in this area
- There is public support for a community center in this area
- Commercial center improvement/redevelopment efforts should include recruiting an upscale chain grocery store (Whole Foods) as an anchor tenant
- New commercial development is not appropriate for this area, the focus should be on redevelopment of existing centers

Appendix 5  
Stakeholders Interviews

## **Holcomb Bridge East Stakeholder Interviewees**

- Greg Hagan - Branch Properties - Holcomb 400 Center
- Charlie Roberts, Anthony Shurtz and Ellen Gleason - Roberts Properties, Inc.
- Roger Wise - Horseshoe Bend HOA
- Steve Bender - Kimberly Clark
- Gay Malony - Martins Landing HOA
- Jim Cowart, Marc Fritz, and Joel Griffin - representing undeveloped property at Scott and Eves in Centennial Node
- Lonnie Mimms - Mimms Enterprises - old Wal-Mart Center
- Steve Vasen - The Pinnacle Company
- Stacy Loftin - Northcliff HOA
- Early Muntzing and Joyce Gross - Muntzing Sattelle - Rivermont Square Shopping Center

Appendix 6  
Market Analysis Tables



**GA 400/OLD ALABAMA ACTIVITY CENTER**

**Transit-Oriented Development Town Center**

Total Acres 33  
 Acres net of buffers/greenspace 3

**Existing Development**

Retail  
 Apartments (Kings Ridge) 322 units

**Fulton County Tax Value**

Total 12,350,700  
 Per acre 374,264

	<i>Density</i>	<i>Number of Units</i>	<i>Unit Size</i>	<i>Total S.F.</i>	<i>Average Unit Value</i>	<i>Total Value</i>	<i>Estimated Land Value</i>
<b>Option A: Commercial Town Center</b>							
Midrise Office (5-6 story)				400,000	110	44,000,000	8,800,000
Retail				40,000	110	4,400,000	880,000
Townhomes	2/acre	66	2,000	132,000	300,000	19,800,000	3,960,000
Total Development	2/acre	66		572,000		68,200,000	13,640,000
Total FAR/ Value per Acre	0.14					2,066,667	
<b>Option B: Residential Town Center</b>							
Midrise office				150,000	110	16,500,000	3,300,000
Retail				20,000	90	1,800,000	360,000
Stacked condos/apartments	6/acre	198	1,200	237,600	150,000	29,700,000	5,940,000
Townhouses	2/acre	66	2,000	132,000	300,000	19,800,000	3,960,000
Total	8/acre	264		539,600		67,800,000	13,560,000
Total FAR/Value per Acre	0.13					2,054,545	

Source: Bleakly Advisory Group

**GA 400/OLD ALABAMA ACTIVITY CENTER**

**Expanded Kings Market Site**

Total Acres	22
Acres net of buffers/greenspace	20

**Existing Development**

Retail	336,000
Other	-
Total	336,000

**Fulton County Tax Value**

Total	23,100,000
Per acre	1,067,301

	<i>Density</i>	<i>Number of Units</i>	<i>Unit Size</i>	<i>Total S.F.</i>	<i>Average Unit Value</i>	<i>Total Value</i>	<i>Estimated Land Value</i>
<b>Option A: Vertical Mixed Use</b>							
Retail	.21 FAR			336,000	110	36,960,000	7,392,000
Residential/lofts	18/acre	396	1,200	475,200	200,000	79,200,000	15,840,000
Total Development		396		811,200		116,160,000	23,232,000
Total FAR/ Value per Acre	0.53					5,280,000	
<b>Option B: Horizontal Mixed Use</b>							
Retail	.07 FAR			260,000	110	28,600,000	5,720,000
Office/other commercial	.035 FAR			120,000	110	13,200,000	2,640,000
Townhomes	6/acre	132	2,000	264,000	300,000	39,600,000	7,128,000
Stacked condos	8/acre	176	1,200	211,200	220,000	38,720,000	7,744,000
Total	14/acre	308		644,000		120,120,000	23,232,000
Total FAR	0.42					5,460,000	
<b>Option C: Residential Mixed Use</b>							
Retail				50,000	110	5,500,000	1,100,000
Townhomes	8/acre	176	2,000	352,000	300,000	52,800,000	10,560,000
Stacked condos	14/acre	308	1,200	369,600	200,000	61,600,000	12,320,000
Total	22/acre	484		771,600		119,900,000	23,980,000
Total FAR	0.51					5,450,000	

Source: Bleakly Advisory Group

**GA 400/OLD ALABAMA ACTIVITY CENTER**

**Holcomb 400**

Total Acres	12
Acres net of buffers/greenspace	11

**Existing Development**

Retail	108,000
Other	-
Total	108,000

**Fulton County Tax Value**

Total	10,557,900
Per acre	894,737

	<i>Density</i>	<i>Number of Units</i>	<i>Unit Size</i>	<i>Total S.F.</i>	<i>Average Unit Value</i>	<i>Total Value</i>	<i>Estimated Land Value</i>
<b>Option A: Mixed Use</b>							
Retail/office	0.28			108,000	110	11,880,000	2,376,000
Stacked condos	16/acre	192	1,200	230,400	220,000	42,240,000	8,448,000
Total Development	16/acre	192		338,400		54,120,000	10,824,000
Total FAR/ Value per Acre	0.86					4,510,000	
<b>Option B: Residential Mixed Use</b>							
Retail	0.05			20,000	110	2,200,000	440,000
Stacked condos	20/acre	240	1,200	288,000	220,000	52,800,000	10,560,000
Total	20/acre	240		308,000		55,000,000	11,000,000
Total FAR/Value per Acre	0.79					4,583,333	

Source: Bleakly Advisory Group

**GA 400/OLD ALABAMA ACTIVITY CENTER**

**Holcomb Bridge Strip Retail**

Total Acres 6  
 Acres net of buffers/greenspace 5

**Existing Development**

Retail 60,000  
 Other -  
 Total 60,000

**Fulton County Tax Value**

Total 6,532,500  
 Per acre 1,088,750

	<i>Density</i>	<i>Number of Units</i>	<i>Unit Size</i>	<i>Total S.F.</i>	<i>Average Unit Value</i>	<i>Total Value</i>	<i>Estimated Land Value</i>
<b>Option A: Mixed Use</b>							
Retail/office	0.23			60,000	110	6,600,000	1,320,000
Stacked condos	22/acre	132	1,200	158,400	200,000	26,400,000	5,280,000
Total Development	22/acre	132		218,400		33,000,000	6,600,000
Total FAR/ Value per Acre	0.84					5,500,000	
<b>Option B: Residential Mixed Use</b>							
Retail	0.08			20,000	110	2,200,000	440,000
Stacked condos	24/acre	144	1,200	172,800	220,000	31,680,000	6,336,000
Total	24/acre	144		192,800		33,880,000	6,776,000
Total FAR/Value per Acre	0.74					5,646,667	

Source: Bleakly Advisory Group

**GA 400/OLD ALABAMA ACTIVITY CENTER**

**Charter School Site**

Total Acres	9
Acres net of buffers/greenspace	8

**Existing Development**

Retail	80,000
Other	-
Total	80,000

**Fulton County Tax Value**

Total	6,947,400
Per acre	771,956

	<i>Density</i>	<i>Number of Units</i>	<i>Unit Size</i>	<i>Total S.F.</i>	<i>Average Unit Value</i>	<i>Total Value</i>	<i>Estimated Land Value</i>
<b>Option A: Mixed Use</b>							
Retail/office	0.20			80,000	110	8,800,000	1,760,000
Stacked condos	15/acre	135	1,200	162,000	200,000	27,000,000	5,400,000
Total Development	15/acre	135		242,000		35,800,000	7,160,000
Total FAR/ Value per Acre	0.62					3,977,778	
<b>Option B: Residential Mixed Use</b>							
Retail	0.08			30,000	110	3,300,000	660,000
Stacked condos	16/acre	144	1,200	172,800	220,000	31,680,000	6,336,000
Total	16/acre	144		202,800		34,980,000	6,996,000
Total FAR/Value per Acre	0.52					3,886,667	

Source: Bleakly Advisory Group

**GA 400/OLD ALABAMA ACTIVITY CENTER**

**Expanded Holcomb Woods**

Total Acres	35
Acres net of buffers/greenspace	32

**Existing Development**

Retail	226,000
Other	-
Total	226,000

**Fulton County Tax Value**

Total	24,370,000
Per acre	687,646

	<i>Density</i>	<i>Number of Units</i>	<i>Unit Size</i>	<i>Total S.F.</i>	<i>Average Unit Value</i>	<i>Total Value</i>	<i>Estimated Land Value</i>
<b>Option A: Mixed Use</b>							
Retail	.21 FAR			226,000	110	24,860,000	4,972,000
Stacked condos	8/acre	280	1,200	336,000	200,000	56,000,000	11,200,000
Townhomes	4/acre	140	2,000	280,000	300,000	42,000,000	8,400,000
Total Development	12/acre	420		562,000		122,860,000	24,572,000
Total FAR/ Value per Acre	0.37					3,510,286	
<b>Option B: Residential Mixed Use</b>							
Retail	.07 FAR			50,000	110	5,500,000	1,100,000
Office/other commercial	.035 FAR			50,000	110	5,500,000	1,100,000
Townhomes	6/acre	210	2,000	420,000	300,000	63,000,000	11,340,000
Stacked condos	8/acre	280	1,200	336,000	220,000	61,600,000	12,320,000
Total	14/acre	490		520,000		135,600,000	25,860,000
Total FAR	0.34					3,874,286	

Source: Bleakly Advisory Group

**CENTENNIAL ACTIVITY AREA**

**Conservation Residential**

Total Acres	49
Acres net of buffers/greenspace	25

**Existing Development**

Retail	-
Other	-
Total	-

**Fulton County Tax Value**

Total	880,000
Per acre	17,959
Assumed Sales Value/Acre	120,000

	<i>Density</i>	<i>Number of Units</i>	<i>Unit Size</i>	<i>Total S.F.</i>	<i>Average Unit Value</i>	<i>Total Value</i>	<i>Estimated Land Value</i>
<b>Option A: Cluster Single-family Housing</b>							
Cluster single family housing	1.0/acre	50	2,500	125,000	350,000	17,500,000	3,500,000
Total Development				125,000		17,500,000	3,500,000
Total FAR/ Value per Acre	0.10					357,143	
<b>Option B: Single-family Detached Housing</b>							
<b>Single-family detached housing</b>	<b>.5/acre</b>	<b>25</b>	<b>3,000</b>	<b>75,000</b>	<b>600,000</b>	<b>15,000,000</b>	<b>3,000,000</b>
Total				75,000		15,000,000	3,000,000
Total FAR/Value per Acre	0.06					306,122	

Source: Bleakly Advisory Group

**NESBITT FERRY ACTIVITY CENTER**

**Expanded Tree Ridge Site**

Total Acres 44  
 Acres net of buffers/greenspace 44

**Existing Development**

Retail 20,000  
 Apartments 360 units

**Fulton County Tax Value**

Total 22,079,000  
 Per acre 512,630

	<i>Density</i>	<i>Number of Units</i>	<i>Unit Size</i>	<i>Total S.F.</i>	<i>Average Unit Value</i>	<i>Total Value</i>	<i>Estimated Land Value</i>
<b>Option A: Senior Independent living</b>							
Cluster/townhomes	6/acre	264	1800	475,200	250,000	66,000,000	13,200,000
Independent living stacked flats	8/acre	352	1,000	352,000	150,000	52,800,000	10,560,000
Total	14/acre	616		827,200		118,800,000	23,760,000
Total FAR/ Value per Acre	0.32					2,700,000	
<b>Option B: Medical Office/Senior Housing</b>							
Medical Office				150,000	110	16,500,000	3,300,000
Cluster/townhomes	6/acre	264	1,800	475,200	250,000	66,000,000	11,880,000
Independent living stacked flats	6/acre	264	1,000	264,000	150,000	39,600,000	7,920,000
Total	12/acre	528		625,200		122,100,000	23,100,000
Total FAR/Value per Acre	0.24					2,775,000	
<b>Option C: Senior Continuum of Care</b>							
Senior Apartments	6/acre	264	1,000	264,000	120,000	31,680,000	6,336,000
Cluster/townhomes	4/acre	176	2,000	352,000	250,000	44,000,000	8,800,000
Assisted living	6/acre	264	600	158,400	150,000	39,600,000	7,920,000
Total	16/acre	704		774,400		115,280,000	23,056,000
Total FAR/Value per Acre	0.30					2,620,000	

Source: Bleakly Advisory Group



Appendix 7  
Median Location Summary

**To:** Robert & Company, Inc.

**From:** Richard Fangmann, Day Wilburn Associates, Inc. (DWA)

**Date:** May 4, 2005

**Subject:** Holcomb Bridge Road – Proposed Landscaped Median Locations

**cc:** Rick Day, DWA

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The section of Holcomb Bridge Road east of SR 400 is being examined for land use and transportation system improvements by Robert and Company, Inc. (RAC) as part of the Holcomb Bridge Road East Revitalization Study. One aspect of the revitalization effort is to enhance the aesthetics of the corridor. Installation of median sections along Holcomb Bridge Road is being considered because these treatments serve the dual role of enhancing corridor aesthetics and controlling and organizing traffic flow. DWA examined preliminary locations for landscaped medians provided by RAC. These locations were considered in regards to traffic service and median break spacing compared to criteria in the Georgia Department of Transportation (GDOT) Regulations for Driveway and Encroachment Control (GDOT Driveway Manual), 2004. The following paragraphs summarize the findings of this examination.

### **General Guidance on Landscaped Median Application**

Median applications take a variety of forms along state routes in Georgia. The paragraphs below provide general guidance on median application.

#### Spacing of Median Breaks

The GDOT Driveway Manual provides guidance regarding the spacing of median breaks, where medians are installed. This document indicates a desirable spacing of 1,320' for median crossovers with a minimum spacing of 660' (45 mph or less). The minimum signal spacing indicated is 1,000'.

#### Typical Median Size

Median size can vary by application and specific design. GDOT has a typical urban median width of 20' to 24'. This allows the median to extend to the intersection with an 8' to 12' median nose and 12' left turn lane. The 8' allows for a 2' gutter section with a 4' raised median nose. When medians are landscaped, the median nose typically remains concrete. In sections with limited right-of-way, GDOT has used a narrow monolithic concrete median (typically 4') which is higher than a standard curb section to enhance its deflection capabilities. The existing median section along Holcomb Bridge Road, from Market Boulevard to Old Alabama Road, is this type of median.

Robert & Company, Inc.  
May 4, 2005

### Median Design to Accommodate U-Turns

When medians are installed along a roadway, they limit access to some properties to right-in and right-out only. This requires drivers to make u-turns to access some movements. In order to design for u-turns on a four-lane road, it is usually necessary to widen at the intersections to allow for the turning radius. Designing for this u-turning capability will require additional width at each signalized intersection at a minimum. In conjunction with adding this width, it would be desirable to add the 8' necessary for to extend the median nose to the intersection.

### Drainage Issues with Median Retrofit

When medians are added to roads on which they were not part of the original design, appropriate drainage design is important. In most cases, addition of the median will require installation of drainage structures in the median area, particularly in curve sections. Another typical median design feature is the extension of the median nose into the intersection. In retrofitting medians to the existing two-way-left-turn-lane (14' to 16'), the RAC design accomplishes the retrofit without widening the road by not including the median nose. In these cases the drainage would need to be carefully considered to ensure ponding of water does not occur in the intersections. In addition, if adjacent driveways are present near the intersection, they would need to be treated to ensure they serve right-in and right-out only where the median is not present. Where planned medians do not extend to the intersections, the potential drainage and design implications should be discussed with GDOT and a design variance would be required.

### Maintenance of Median Landscaping

GDOT typically requires local government agencies to maintain landscaping in medians along state routes. A maintenance agreement may be required by GDOT.

### Landscape Materials

When landscaping a median area, it is important to design landscaping to allow for adequate sight distance. The sight distance must be maintained for vehicles turning from the main road or side street. In addition, the landscape materials must be compatible with clear zone requirements, particularly with higher speed roads such as Holcomb Bridge Road. These requirements limit the diameter of trees and shrubs to ensure they will break off in a major motor vehicle collision. In addition, use of low maintenance, drought resistant materials is recommended to minimize the danger to workers in the median of high volume roads.

Robert & Company, Inc.  
May 4, 2005

### **Recommendations Regarding Median Placement**

The typical design recommended for median placement along Holcomb Bridge Road includes the following:

- Provide 11' travel lanes.
- Provide 11' turning lanes.
- Provide a 16' landscaped median between median break locations.
- Provide a 4' wide monolithic concrete median nose at intersections.
- Widen at intersections, as needed, to provide appropriate turning radius for u-turns from Holcomb Bridge Road.
- Provide median break spacing that is compliant with the GDOT Driveway manual.

The sections below provide comments (organized by Holcomb Bridge Road segment) regarding the proposed landscaped median locations provided by RAC.

#### I-75 to Market Boulevard

- Existing concrete median section with median nose at Market Boulevard.
- Retain existing median.
- Wide concrete portion could be converted to landscaped median.

#### Market Boulevard to Old Alabama Road

- Existing narrow concrete median.
- Retain existing narrow (4') median.
- Landscaped median cannot be added without widening median.

#### Old Alabama Road to Holcomb Woods Parkway

- Existing two-way-left-turn-lane.
- Compatible with median retrofit.
- A median break should be provided for fire station access. This median break should provide channelization in the median area to allow left turns into and out of the fire station only (no median break for access north of Holcomb Bridge Road).

Robert & Company, Inc.  
May 4, 2005

Holcomb Woods Parkway to Martins Landing

- Existing two-way-left-turn-lane with property access on north side of Holcomb Bridge Road only.
- Compatible with median retrofit.
- No median breaks would be permitted in this area.

Martins Landing to Calibre Creek

- Existing two-way-left-turn-lane with no parcel access.
- Compatible with median retrofit.
- No median breaks are needed.

Calibre Creek to Eves Road

- Existing four lane section with no parcel access.
- Compatible with 4' concrete median with 11' travel lanes.
- No median breaks are needed.

Eves Road to Fouts Road

- Existing two-way-left-turn-lane with property access on north side of Holcomb Bridge Road only. Existing driveways are right-in and right-out only.
- Compatible with median retrofit.
- No median breaks are needed.

Fouts Road to Steeplechase West

- Existing two-way-left-turn-lane.
- The section from Fouts Road to Scott Road should not have a median to retain flexibility for queuing of back to back left turns.
- The section from Scott Road to Steeplechase West is compatible with a landscaped median.
- No median breaks would be permitted in this area.

Steeplechase West to Steeplechase East

- Existing two-way-left-turn-lane.
- Compatible with median retrofit.
- No median breaks are needed between Steeplechase West and Champions Green Parkway.

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- At most two median breaks would be permitted between Champions Green Parkway and Holcomb Bridge Middle School.

Steeplechase East to Nesbit Ferry Road

- Existing two-way-left-turn-lane.
- Compatible with median retrofit.
- No median breaks would be permitted in this area.

We hope this information is helpful as you continue to plan the landscaped median treatments along Holcomb Bridge Road. As always, do not hesitate to call me if you have any questions regarding these findings.