



HISTORIC
GATEWAY

ROSWELL, GEORGIA

MASTER PLAN

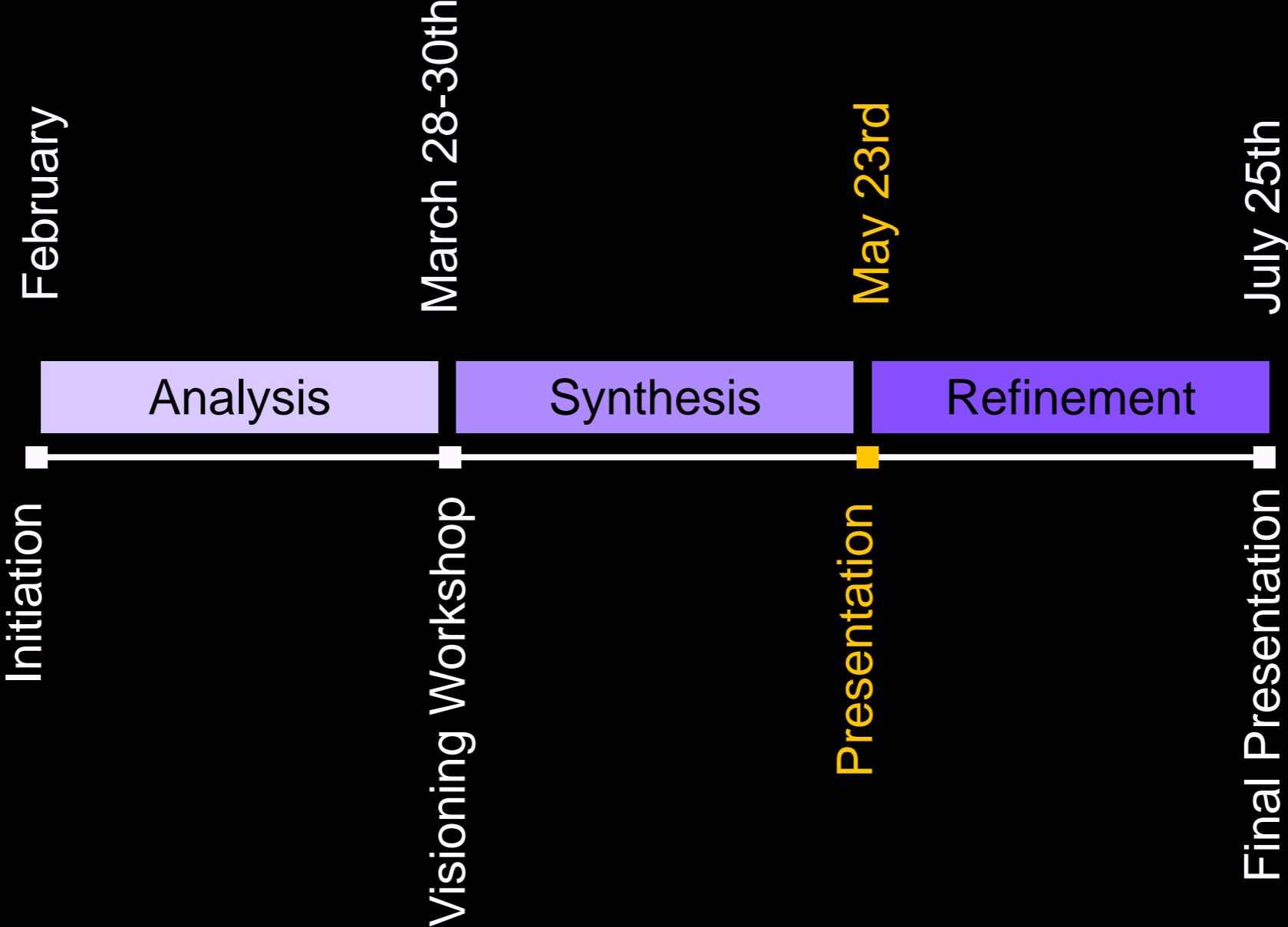
DRAFT REPORT
MAY 2012



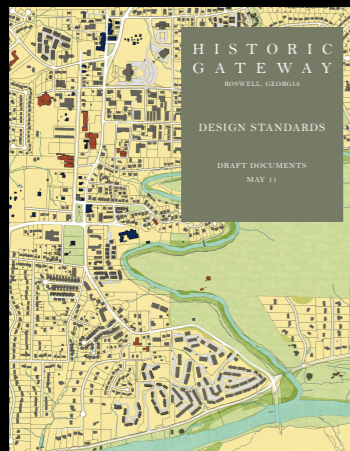
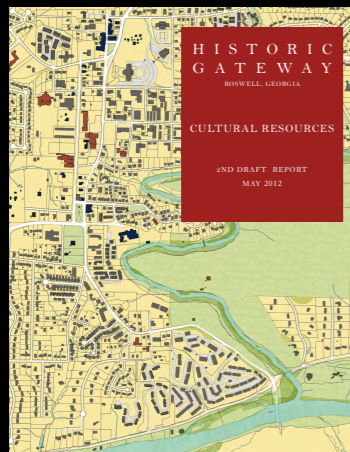
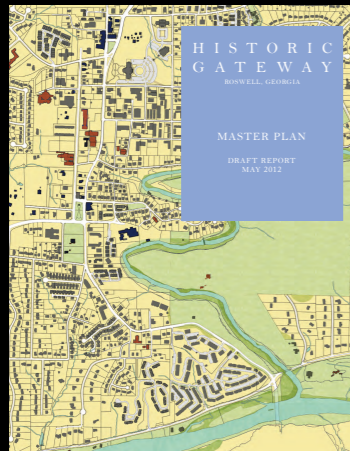
Draft Master Plan Presentation

Scott Ball | Senior Project Manager | Duany Plater-Zyberk & Company | May 23, 2012

PLANNING PROCESS



Analysis



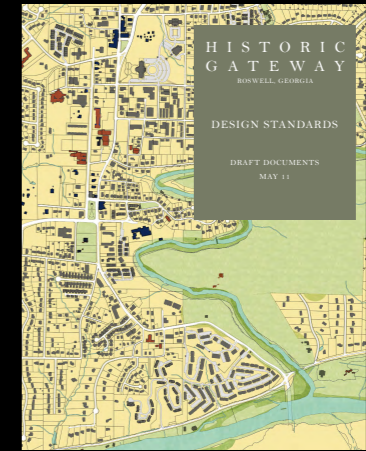
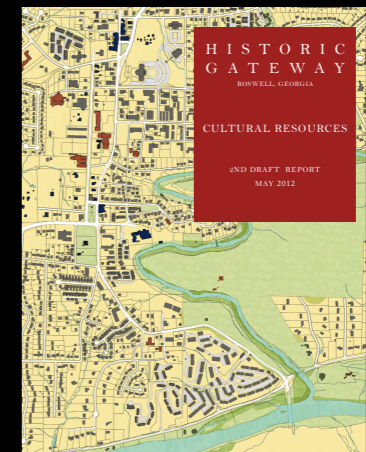
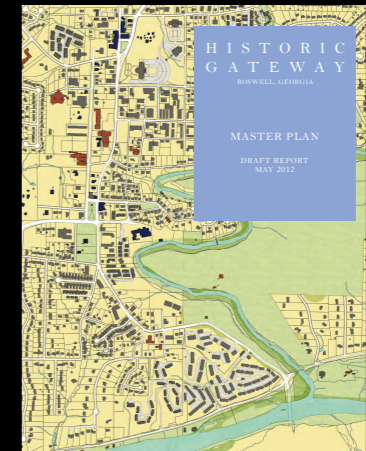
Document Base Conditions
Establish Study Framework

Synthesis



Establish Vision
Update Framework
Make Recommendations

Refinement



Refine Recommendations
Finalize Framework
Adopt as Master Plan



Why Plan in a Historic Preservation Area?

- Things change, context shifts, and history evolves;
- The character of the corridor has been compromised with past focus on the automobile;
- In order to re-establish a context for the area as a vibrant multimodal center of heritage, culture, business and neighborhood life that is friendly to commuters, visitors, and residents alike.

ROSWELL TOWN CENTER/ ATLANTA STREET CORRIDOR STUDY



APRIL, 2008

GOODY CLANCY

Funded by the Atlanta Regional Commission
and the City of Roswell, Georgia

STREET SMARTS | RCLCo | MARY MEANS + ASSOCIATES

CONTINUITY

**This Plan Updates the 2008
Livable Centers Initiative Atlanta
Street Corridor Plan**

**LCI Plan was Approved but Not
Adopted by the City**

**Update Recommendation:
Clarify Status by adopting the LCI
Plan or by supplanting the LCI
Plan with the adopted Update.**

CONTINUITY

LCI Recommendations:

- Create village identities
- Promote mixed-use development
- Create balanced transportation solutions
- Design for pedestrian-friendly environments
- Refocus and refine historic preservation efforts

Update Recommendation:

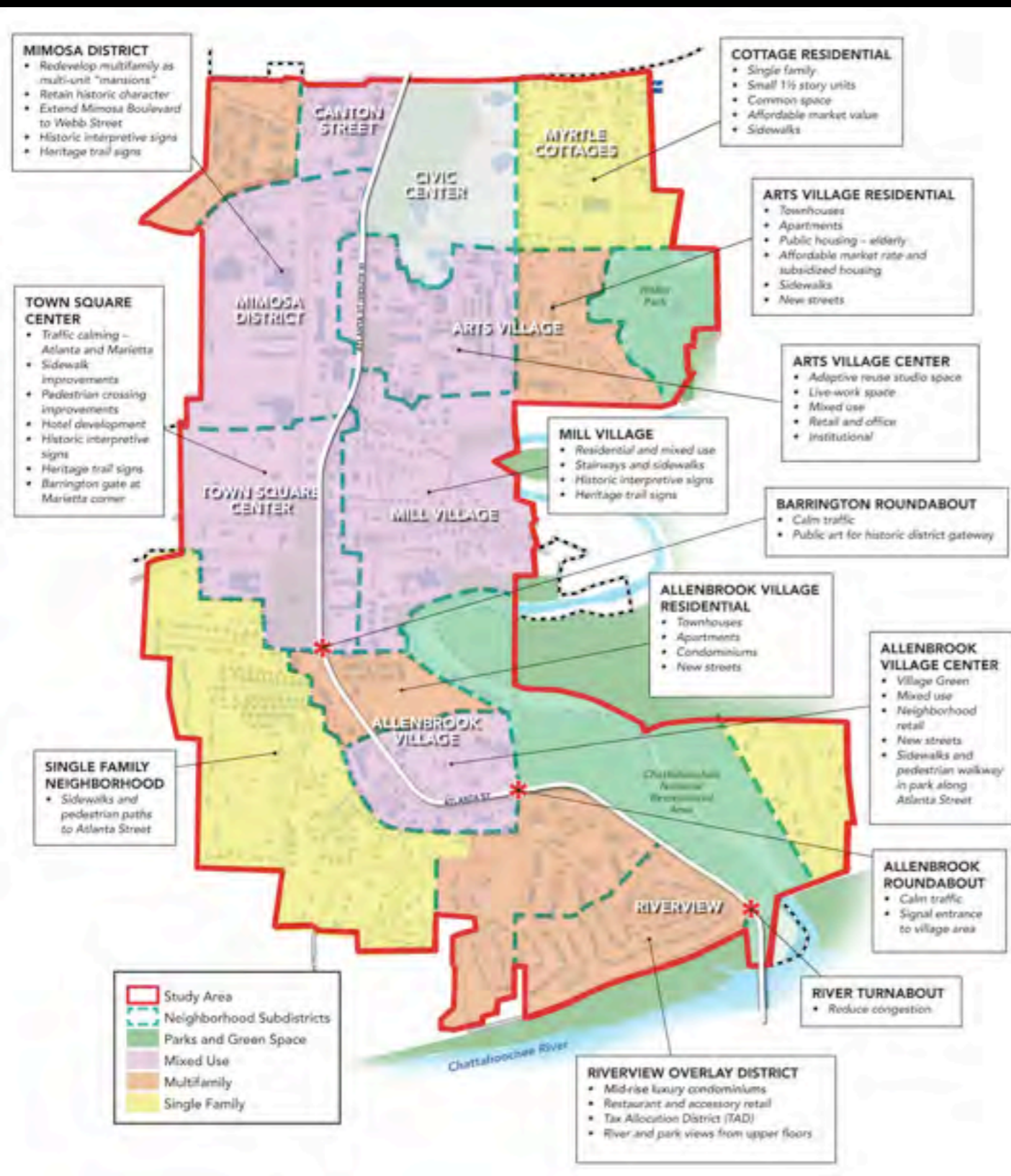
Update Report Should Supplant LCI Plan
Extend update to cover entire Historic
District Gateway



Create Village Identities

LCI Recommendations:

- Create village identities
- Promote mixed-use development
- Create balanced transportation solutions
- Design for pedestrian-friendly environments
- Refocus and refine historic preservation efforts



Update Recommendation:
 Revise "Village" boundaries to reflect pedestrian-scaled neighborhood relationships.

LCI Recommendations:

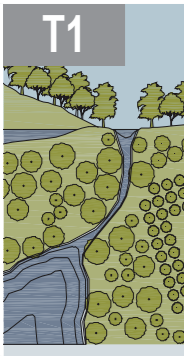
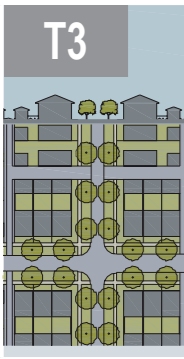
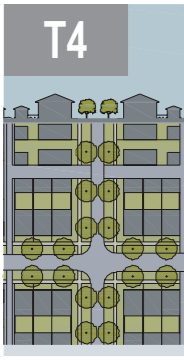
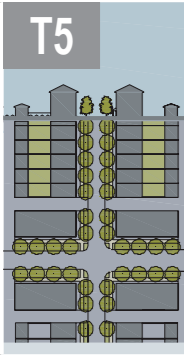
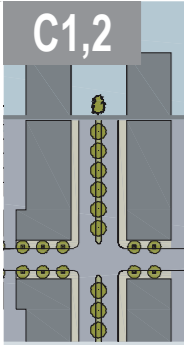
- **Create village identities**
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Update Recommendation:

Emphasize three Village Centers that each serve one or more historic zones.



TABLE 1A. ZONING DISTRICT DESCRIPTIONS

 <p>T1</p>	<p>B-3.1 NATURAL ZONE This zone consists of lands approximating or reverting to a wilderness condition, including lands unsuitable for settlement due to topography, hydrology or vegetation.</p>
 <p>T3</p>	<p>B-3. 3 NEIGHBORHOOD GENERAL ZONE This zone consists of a medium to low-density mix of primarily cottages and houses with some shop fronts. Buildings are primarily one-story and accommodate housing and neighborhood services and cafes. Blocks are larger and loosely built with primarily swales, and buildings are set back from the street. Sidewalks and pedestrian paths are narrow, landscaping is naturalistic and parking is accommodated within the lot or parallel on street.</p>
 <p>T4</p>	<p>B-3.2 NEIGHBORHOOD CENTER ZONE: This zone consists of medium density, primarily corner shop front and 2-story mixed-use buildings that accommodate retail, neighborhood services, offices, artisanal shops and upper story apartments. Blocks are larger but regularly built, usually with street curbs, and buildings are near the street. Sidewalks are wide, tree planting is regular and parking is predominantly in adjacent lots or on the street.</p>
 <p>T5</p>	<p>T5 TOWN CENTER ZONE This zone consists of higher density, minimum 2-story mixed-use buildings that accommodate larger retail, hospitality services, offices and upper story apartments. Blocks are tightly built, streets are curbed and buildings are close to the street. Sidewalks are wide, tree planting is regular and parking is in the rear.</p>
 <p>C1,2</p>	<p>C-1 & C-2 CIVIC ZONES This zone consists of Government (C-1), Educational or Religious Institutions (C-3) uses that serve the public good. The architecture and urbanism is uniquely designed.</p>

LCI Recommendations:

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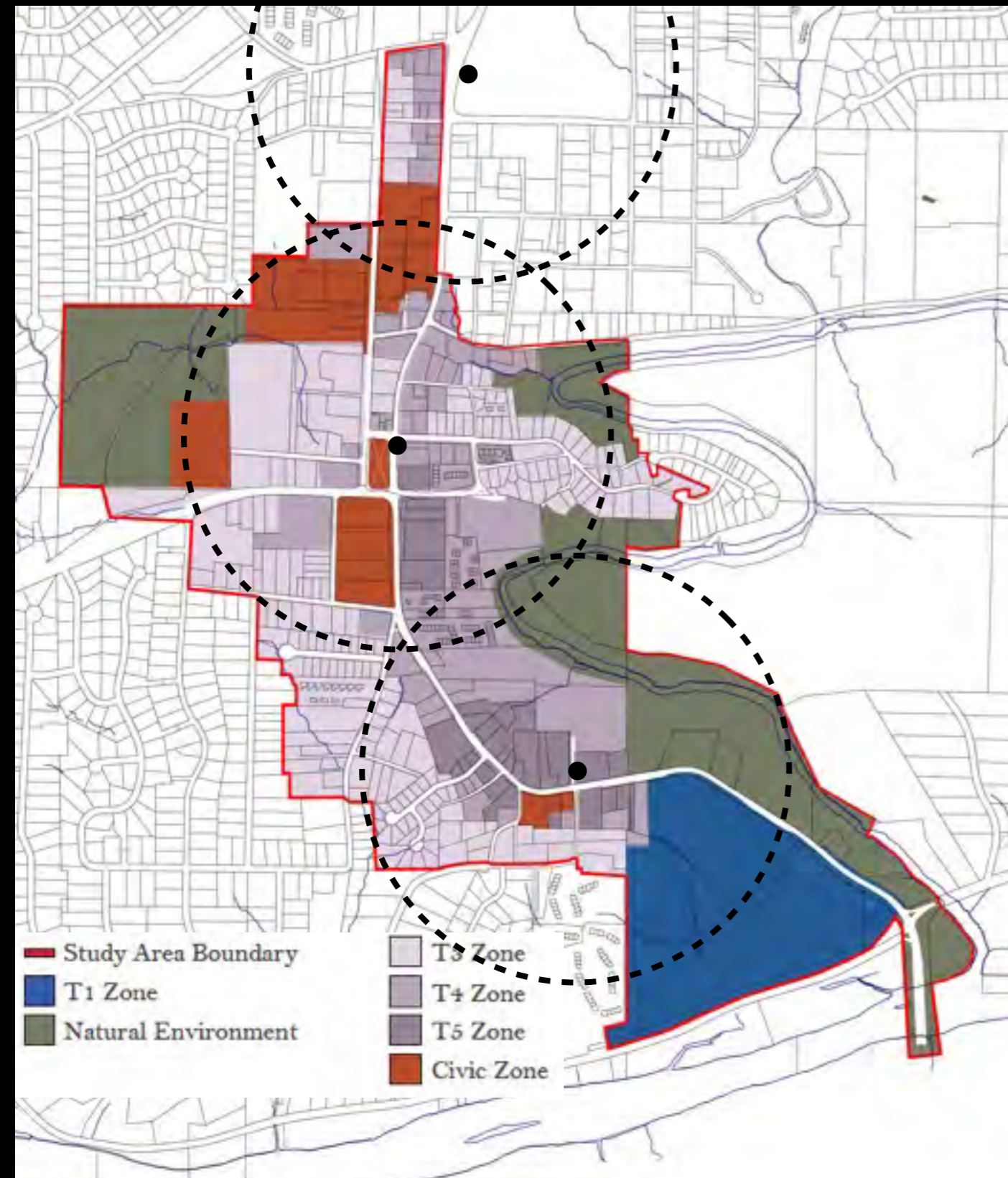
Update Recommendation: Utilize Transect in structuring Villages

LCI Recommendations:

- **Create village identities**
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Update Recommendation:

Adopt a Regulating Plan to define individual village identities and inter-village relationships.



Promote Mixed-Use Development

CONNECT THROUGHFARES AND PROVIDE FOR FUTURE TRANSIT
 Atlanta Street commands a wide right-of-way that could one day accommodate rapid bus lanes or light rail. Parallel parking can also be added as the street is redeveloped, both to shield pedestrians on the sidewalk, and to support more retail in the corridor.

GENERAL PRINCIPLES
 Connect thoroughfares and accommodate transit



INTRODUCE URBAN BUILDING TYPES AND MIXED USES
 Atlanta Street is home to an abundance of antiquated, low grade strip retail buildings. These buildings do not blend well with the surrounding residential neighborhoods, nor do they present an appealing face to the through traffic. The introduction of parking garages around the town square will allow greater density, while the introduction of an increased variety of building types (apartments, hotels, offices, with shops and restaurants along the ground floors) will create a more vibrant, better defined town center destination of the square.

GENERAL PRINCIPLES
 Allow greater density at the town center intersection to anchor it as a center.



Like the intersection illustrated to the left, Atlanta Street has four lanes and intersects another regional road (Marietta Highway) at the Town Center Square. The intersection is hostile to pedestrians and bicyclists, with oversized turning radii and limited landscaping. Oversized and underutilized strip retail dominates the streetscape.

The first phase of the redevelopment includes only streetscape changes - reducing lane widths, adding parallel parking, installing sidewalks, planting strips, and visible pedestrian crossings. This sets the stage for the building replacement and infill.

The last phase includes the redevelopment of the obsolete buildings to form a continuous, properly scaled frontage along the new main street and the repaired intersection.



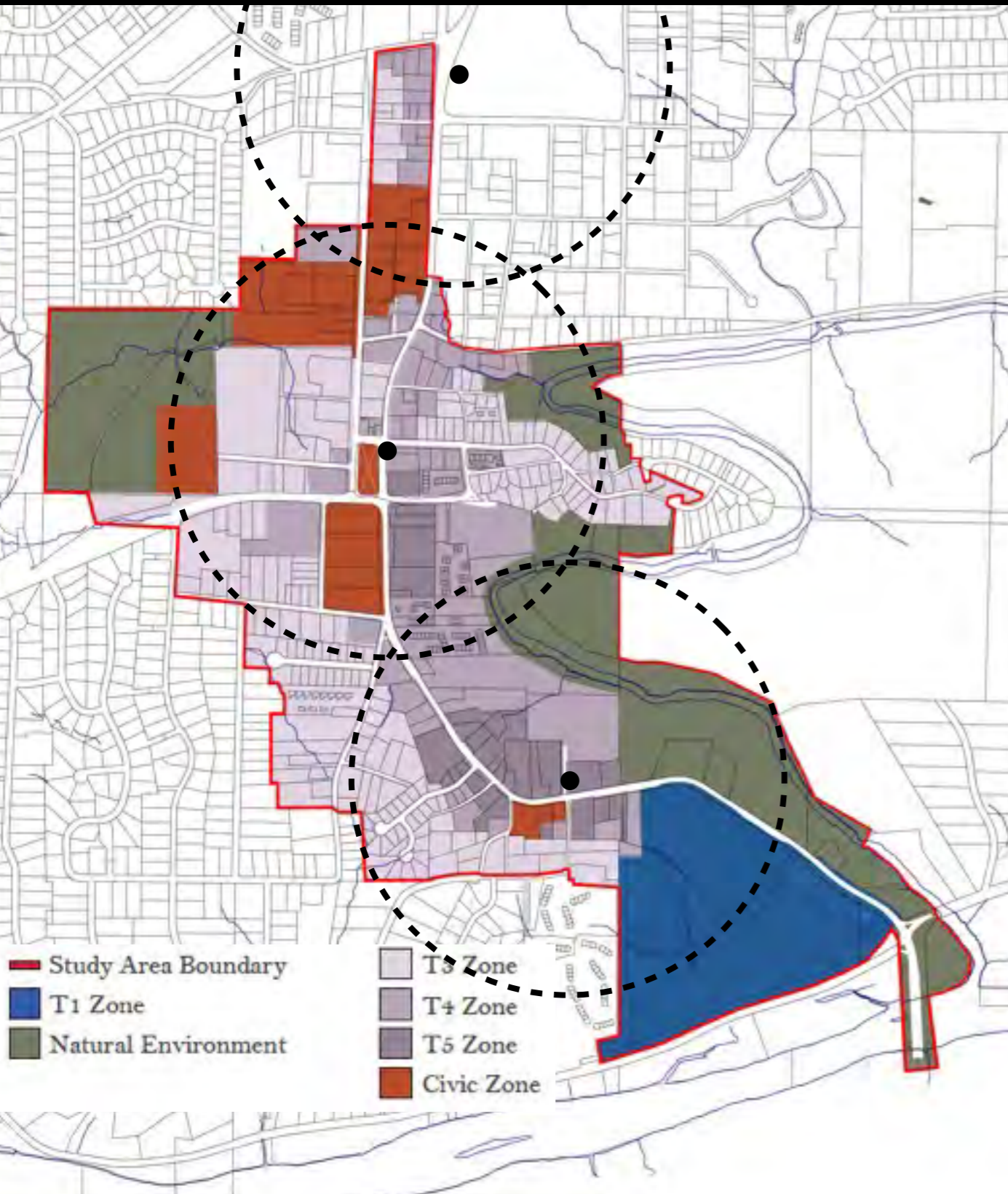
LCI Recommendations:

- Create village identities
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Update Recommendation: Establish Redevelopment Principles

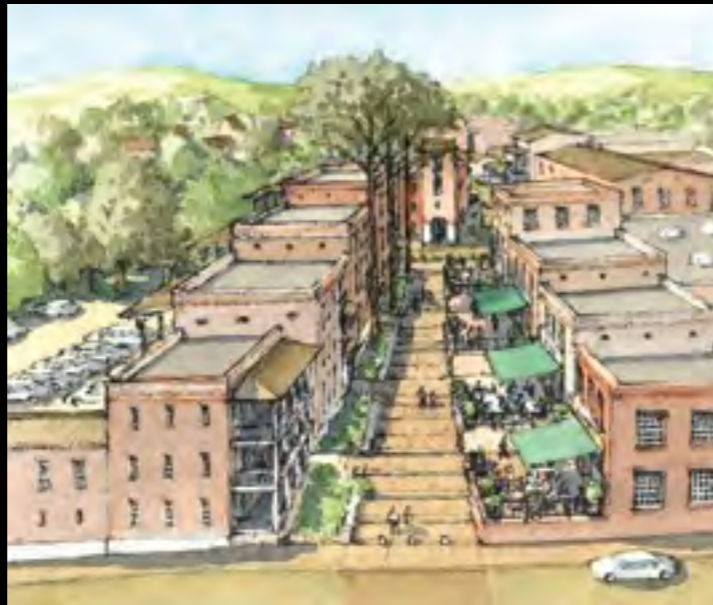
LCI Recommendations:

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Update Recommendation:

Prioritize three centers for more intense application of Redevelopment Principles.



LCI Recommendations:

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Update Recommendation:

Develop the three centers, based on Redevelopment Principles with different characters: Civic, Town Center, Neighborhood Center.

	T3	T4	T5
a. RESIDENTIAL	Restricted Residential: The number of dwellings on each lot is restricted to one within a principal building and one within an accessory building, with 1.5 parking places for each. Both dwellings shall be under single ownership.	Limited Residential: The number of dwellings on each lot is limited by the requirement of 1.5 parking places for each dwelling.	Limited Residential: The number of dwellings on each lot is limited by the requirement of 1.5 parking places for each dwelling.
b. LODGING	Restricted Lodging: The number of bedrooms available on each lot for lodging is limited by the requirement of 1.0 assigned parking place for each bedroom, up to five, in addition to the parking requirement for the dwelling. The lodging must be owner occupied. Food service may be provided in the morning. The maximum length of stay shall not exceed ten days.	Restricted Lodging: The number of bedrooms available on each lot for lodging is limited by the requirement of 1.0 assigned parking place for each bedroom, up to five, in addition to the parking requirement for the dwelling. The lodging must be owner occupied. Food service may be provided in the morning. The maximum length of stay shall not exceed ten days.	Limited Lodging: The number of bedrooms available on each lot for lodging is limited by the requirement of 1.0 parking place for each bedroom, up to twelve, in addition to the parking requirement for the dwelling. The lodging must be owner occupied. Food service may be provided in the morning. The maximum length of stay shall not exceed fourteen days.
c. OFFICE	Restricted Office: The building area available for office use on each lot is restricted to the first story of the principal or the accessory building or in basements. Parking requirements and maximum number of employees and occupants shall be determined by warrant.	Limited Office: The building area available for office use on each lot is limited to the first story of the principal building and accessory building, and by the requirement of 3.0 assigned parking places per 1,000 square feet of net office space in addition to the parking requirement for each dwelling.	Open Office: The building area available for office use on each lot is limited by the requirement of 2.0 assigned parking places per 1,000 square feet of net office space.
d. RETAIL	N/A	Limited Retail: The building area available for retail use is limited to the first story of buildings at corner locations and by the requirement of 3.0 assigned parking places per 1,000 square feet of net retail space in addition to the parking requirement of each dwelling. Food service shall be further limited to seating no more than 40 patrons.	Limited Retail: The building area available for retail use is limited by the requirement of 2.0 assigned parking places per 1,000 square feet of net retail space. Retail spaces under 1,500 square feet are exempt from parking requirements.
e. CIVIC	See Table 5F	See Table 5F	See Table 5F

LCI Recommendations:

- Create village identities
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- Create balanced transportation solutions
- Design for pedestrian-friendly environments
- Refocus and refine historic preservation efforts

Update Recommendation:

Adopt the Transect-Based use tables guided by the Regulating Plan

	T3	T4	T5	SD	C
a. RESIDENTIAL					
MEDIUM-DENSITY RESIDENTIAL	0	*	0	0	0
MULTI-FAMILY RESIDENTIAL	0	*	0	0	0
SINGLE-FAMILY RESIDENTIAL	*	*	0	0	0
SENIOR HOUSING	*	*	*	0	0
ACCESSORY UNIT	*	*	0	0	0
b. LODGING					
HOTEL (ONE ROOM UNIT)				0	0
INN (UP TO 10 ROOMS)		*	0	0	0
BEDS BREAKFAST (UP TO 10 ROOMS)	*	*	0	0	0
SCHOOL DORMITORY		0	0	0	0
c. OFFICE					
OFFICE BUILDING		*	*	0	0
LABOR WORK UNIT	*	*	*	*	0
d. RETAIL					
OPEN-MARKET BUILDING	0	*	*	*	*
RETAIL BUILDING		*	*	0	0
DISPLAY GALLERY		*	*	0	0
RESTAURANT		*	*	0	0
BOOK		*	*	*	*
POSTAL/ART			*	*	*
e. AGRICULTURE					
LANDSCAPE FIRM					
GREENHOUSE	*	*		*	*
STALL	*			*	*
SHED				*	*
GRASS PASTURE (OVERHEAD)	*	*	*	*	*
WATER TREATMENT	*	*	*	*	*
f. OTHER: AUTOMOTIVE					
GAS STATION			0		
AUTOMOBILE SERVICE					
TRUCK MAINTENANCE					
DRIVE-THROUGH VEHICLE					
g. OTHER: CIVIL SUPPORT					
COURTNEY					0
PERNIAL HOME		*	0		
HOSPITAL					
MEDICAL CLINIC	*	*	*	*	*
h. OTHER: EDUCATION					
COLLEGE		0	0	*	0
HIGH SCHOOL				*	0
MIDDLE SCHOOL		0	0	*	0
ELEMENTARY SCHOOL	0	0	0	*	*
CARE CENTER	*	*	*	*	*
i. OTHER: INDUSTRIAL					
HEAVY INDUSTRIAL FACILITY					
LIGHT INDUSTRIAL FACILITY					
TRUCK DEPOT					
LABORATORY FACILITY					
LABORATORY FACILITY					
WAREHOUSE					
PRODUCT STORAGE					
RAW STORAGE					
HEAVY MANUFACTURING					
LIGHT MANUFACTURING	0			0	

* BY RIGHT
0 BY WARRANT

Create Balanced Transportation Solutions
Design for Pedestrian-Friendly Environments

LCI Recommendations:

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a. (SHARED USE) BICYCLE TRAIL AND BICYCLE PATH		T1	T2	T3	T3	T4	T5				
		Bicycle Trail (BT)		Bicycle Path (BP)							
Bikeway Type		Bicycle Trail (BT)		Bicycle Path (BP)							
Riding Surface Width		6'-12' E		10'-14' E							
Movement		dual direction		dual direction							
Intersection Detailing		signed		signed, signposted							
Bicycle Parking		rack, Bicycle Shelter		rack, Bicycle Shelter, Bicycle Locker							
b. BICYCLE LANE (in T4, T5, T6 recommended for retrofit only)		T2	T3	T4	T5	T6	T2	T3	T4	T5	T6
		Conventional Bicycle Lane (BL)		Bicycle Lane with Bicycle Box (BLB)		Physically Seg. Bicycle Lane (BLP)		Buffered Bicycle Lane (BLB)		Dually Buffered Bicycle Lane (BLDB)	
Bikeway Type		Conventional Bicycle Lane (BL)		Bicycle Lane with Bicycle Box (BLB)		Physically Seg. Bicycle Lane (BLP)		Buffered Bicycle Lane (BLB)		Dually Buffered Bicycle Lane (BLDB)	
Riding Surface Width		5' 8" min. w/ parking, 4' 8" min. w/ parking with traffic or contraflow		5' 8" min each way, 6' 0" depth 14' E		5' 8" w/ 2' 8" min buffer with traffic or dual direction		5' 8" min each way 2' 8" min shared buffer with traffic		5' 8" min each way 2' 8" min shared buffer dual direction	
Movement		signed, dashed, Pigeon-Track colored, Bicycle Box		signed, dashed, Pigeon-Track colored, Bicycle Box		signed, dashed, Pigeon-Track colored, Bicycle Box		signed, Pigeon-Track colored, Bicycle Box		signed, Pigeon-Track colored, Bicycle Box	
Intersection Detailing		signed, dashed, Pigeon-Track colored, Bicycle Box		signed, dashed, Pigeon-Track colored, Bicycle Box		signed, dashed, Pigeon-Track colored, Bicycle Box		signed, Pigeon-Track colored, Bicycle Box		signed, Pigeon-Track colored, Bicycle Box	
Bicycle Parking		rack, Bicycle Shelter, Bicycle Station		rack, Bicycle Shelter, Bicycle Station		rack, Bicycle Shelter, Bicycle Station		rack, Bicycle Shelter, Bicycle Station		rack, Bicycle Shelter, Bicycle Station	
c. SHARED VEHICULAR LANES		T1	T2	T3	T3	T4	T5	T6	T3	T4	
		Shoulder (SL)		Shared Vehicular Lane w/ Shoulder (SL)		Bicycle Boulevard (BB)					
Bikeway Type		Shoulder (SL)		Shared Vehicular Lane w/ Shoulder (SL)		Bicycle Boulevard (BB)					
Riding Surface Width		5' 8" min.		width of Vehicular Lane		width of Vehicular Lane					
Movement		with traffic		with traffic		with traffic					
Intersection Detailing		signed, signposted		signed, signposted		signed, signposted					
Bicycle Parking		signposted, rack, Bicycle Shelter		signposted, rack, Bicycle Shelter		signposted, rack, Bicycle Shelter					

Update Recommendations:
Utilize a wide pallet of bike accommodations calibrated to specific contexts.

In order to reduce the need for additional pavement, make use of shared lanes in safe, slow local streets whenever possible.

LCI Recommendations:

- Create village identities
- Promote mixed-use development
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- Refocus and refine historic preservation efforts



Bicycling Suitability Map



Most streets are very good for bicycling (green)

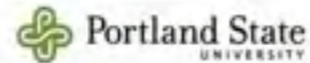
Atlanta Street and Marrietta Highway are very bad for bicycling (red)

Where do people bicycle?

Using GPS to examine the role of infrastructure in improving health



Jennifer Dill, Ph.D.
Associate Professor, Urban Studies & Planning
Director, Oregon Transportation Research & Education Consortium
New Partners for Smart Growth • Seattle, WA • February 4, 2010



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Update Recommendations:

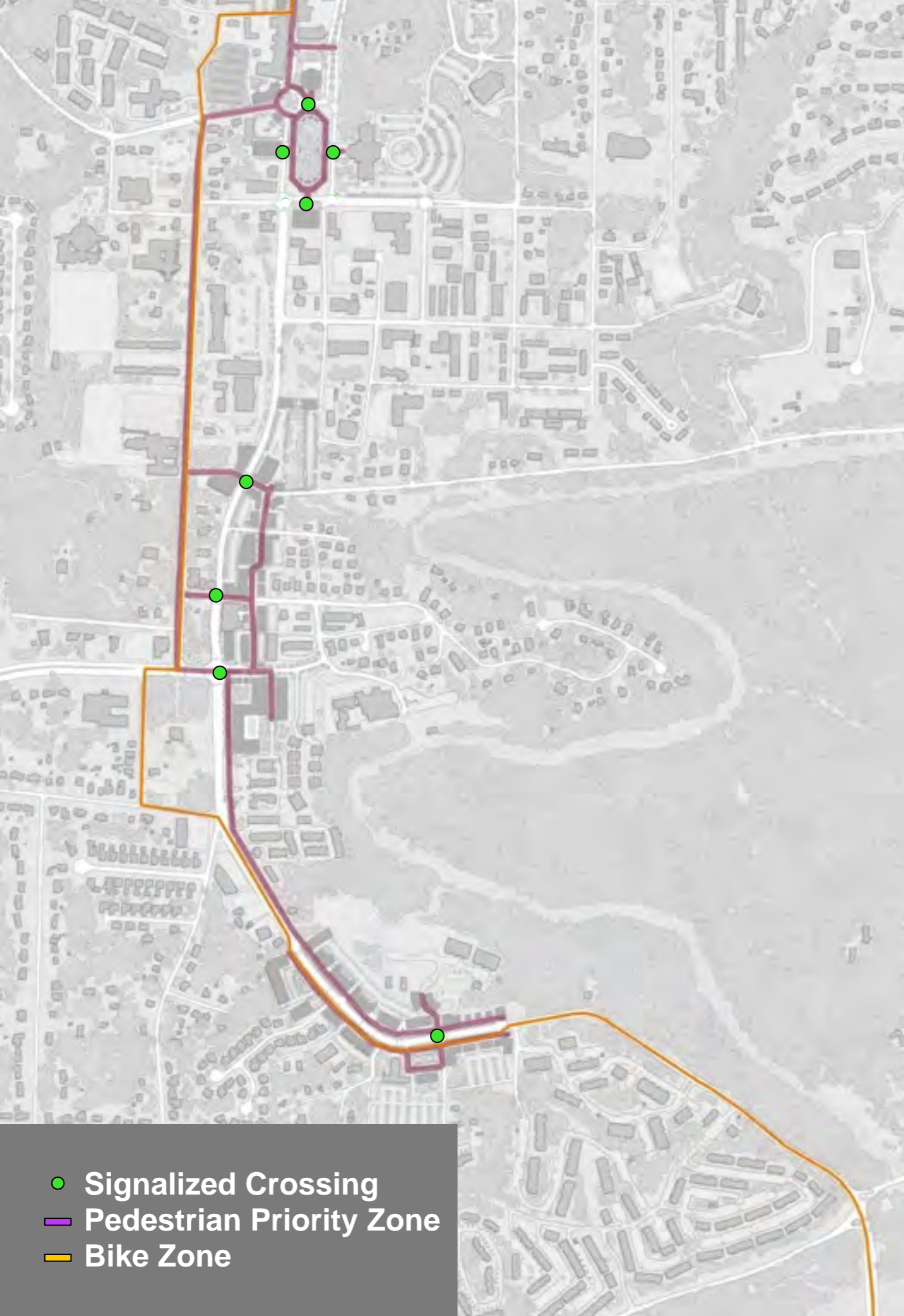
Consider route choice behaviors when planning bike corridors.

Recreational bikers' behavior is different than commuter bike behavior. This is particularly important when considering routes along S. Atlanta down to the river where bike demand is primarily recreational.

Priorities for route choice

	Mean score	
	1=not at all impt, 5=very impt	
	Men	Women
Avoiding streets with lots of vehicle traffic	3.46	3.77
Minimize total distance	3.31	3.73
Riding in a bike lane	2.98	2.97
Reducing wait time due to stop signs/lights	2.59	2.70
Riding on signed bike routes	2.60	2.68
Riding on an off-street bike trail/path	2.19	2.31
Avoiding hills	1.92	2.28
N (trips)	863	762

Excluded from all and exercise trips



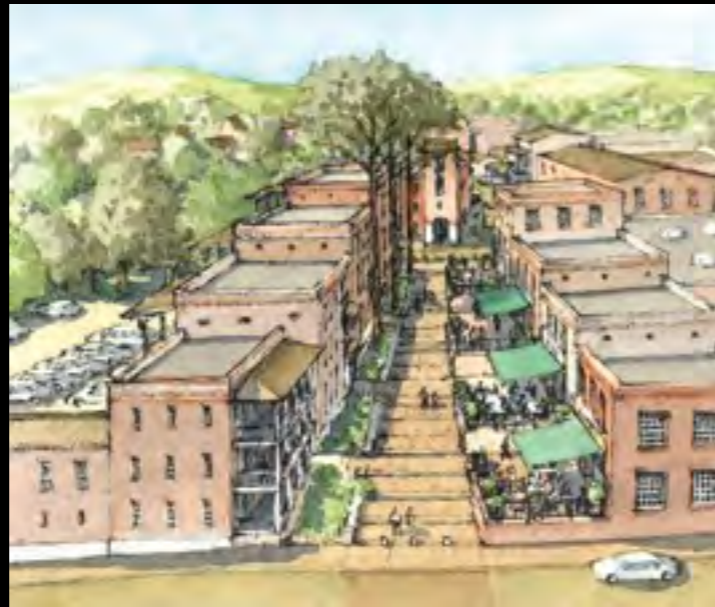
- Signalized Crossing
- Pedestrian Priority Zone
- Bike Zone

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Update Recommendation:

Proposals for integrating bike and pedestrian circulation in the Atlanta Street corridor



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Update Recommendation:

Adopt the group of focus area plans as the Historic District Redevelopment Master Plan.



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Update Recommendation:

Two way boulevard rather than one way split. The boulevard option is better for:

- Commerce
- Cost
- Visual Impact
- Politics
- Access
- Urban Form



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	070	080	090	010	020	030	040	050	060	070	080
PRIVATE LOT LAYER											
OWNERSHIP TYPE											
RESIDENTIAL											
COMMERCIAL											
INDUSTRIAL											
INSTITUTIONAL											
OTHER											
FUNCTION											
RESIDENTIAL											
COMMERCIAL											
INDUSTRIAL											
INSTITUTIONAL											
OTHER											
PRIVATE FRONTAGE TYPE											
RESIDENTIAL											
COMMERCIAL											
INDUSTRIAL											
INSTITUTIONAL											
OTHER											
SETBACK											
RESIDENTIAL											
COMMERCIAL											
INDUSTRIAL											
INSTITUTIONAL											
OTHER											
PUBLIC FRONTAGE LAYER											
CURBING TYPE											
CONCRETE											
ASPHALT											
OTHER											
WALKWAY TYPE											
CONCRETE											
ASPHALT											
OTHER											
PLANTING TYPE											
CONCRETE											
ASPHALT											
OTHER											
TREE PLANTING TYPE											
CONCRETE											
ASPHALT											
OTHER											
TREE PLANTING SHAPE											
CONCRETE											
ASPHALT											
OTHER											
LIGHTING TYPE											
CONCRETE											
ASPHALT											
OTHER											
TRANSPORTATION LAYER											
PARKING LANE											
CONCRETE											
ASPHALT											
OTHER											
VEHICLE FLOW											
CONCRETE											
ASPHALT											
OTHER											
VEHICLE LANE											
CONCRETE											
ASPHALT											
OTHER											
BIKEWAY TYPE											
CONCRETE											
ASPHALT											
OTHER											
BIKEWAY											
CONCRETE											
ASPHALT											
OTHER											
TRANSIT PROVISION											
CONCRETE											
ASPHALT											
OTHER											

Update Recommendation:

Utilize the Atlanta Street Scroll in the design of the road and when road is finished utilize it in finalizing a code for the corridor.

Refocus and Refine Historic Preservation Efforts



LCI Recommendations:

- Create village identities
- Promote mixed-use development
- Create balanced transportation solutions
- Design for pedestrian-friendly environments
- **Refocus and refine historic preservation efforts**

Update Recommendation:

Think of villages and historic zones separately: a single neighborhood or village can contain more than one historic zones.



LCI Recommendations:

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- **Refocus and refine historic preservation efforts**

Update Recommendation:

Consider the role of Zone D in the Historic District. There are a collection of historic buildings in this zone, but not a strong place-context.



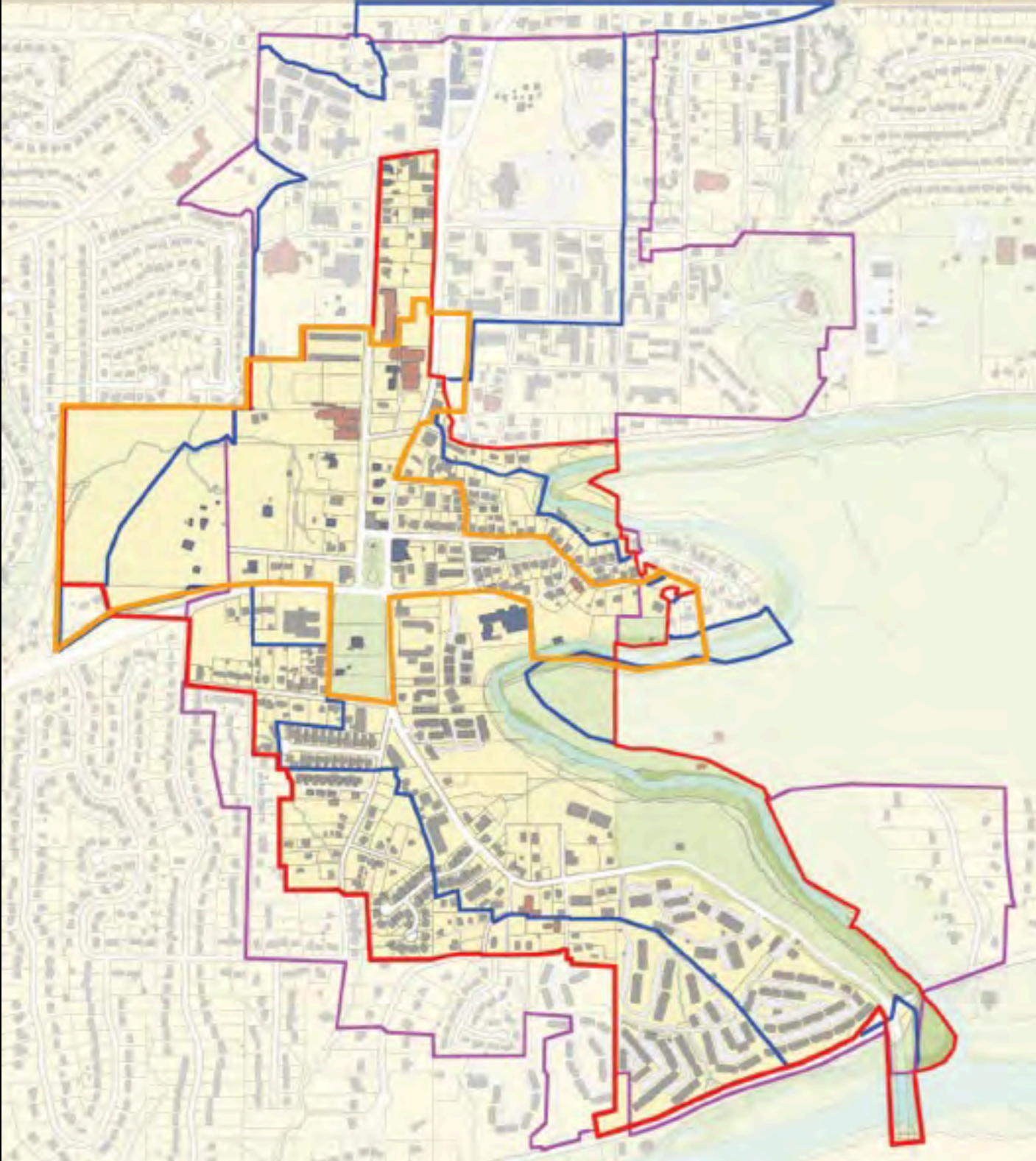


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Update Recommendation:

Adopt Redevelopment Priority Areas within the Historic District. Place less emphasis on building preservation than on place-context repositioning in these areas.



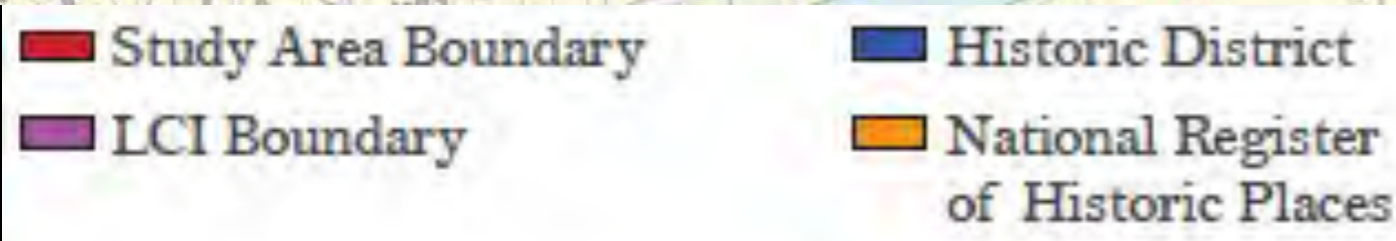
■ Study Area Boundary ■ Historic District
■ LCI Boundary ■ National Register of Historic Places

LCI Recommendations:

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Update Recommendation:

Reinforce the integrity of the Historic District by regularizing study boundaries and Historic District boundaries.




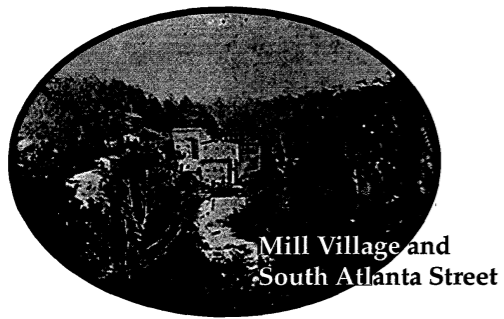
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- Design for pedestrian-friendly environments
- **Refocus and refine historic preservation efforts**

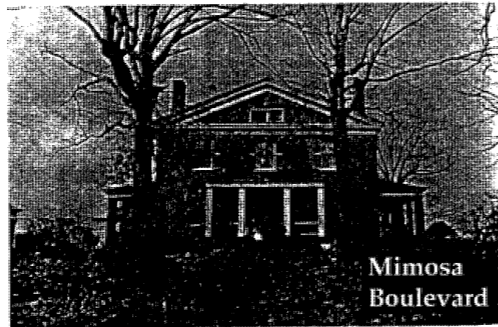
Update Recommendation:

Determine the best boundary and then amend all districts to conform to that boundary


Roswell 
**Historic District
Design Guidelines**



Roswell 
**Historic District
Design Guidelines**



Adopted by Mayor and City Council on December 1, 1997

Roswell 
**Historic District
Design Guidelines**



Adopted by Mayor and City Council on December 1, 1997

LCI Recommendations:

- Create village identities
- Promote mixed-use development
- Create balanced transportation solutions
- Design for pedestrian-friendly environments
- **Refocus and refine historic preservation efforts**

Update Recommendation:

Integrate Historic District Design Guidelines into Zoning Code.

Amend and Update these as necessary, with each update supplanting the preceding version.

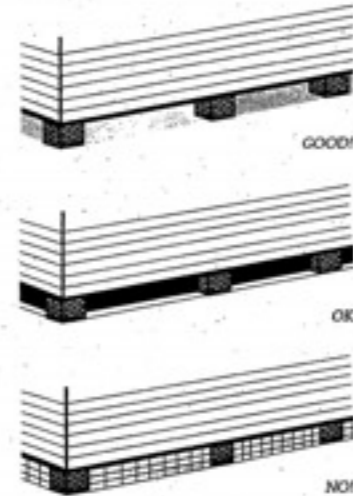
Existing historic windows and details should be maintained and preserved. Historic windows should only be replaced when they cannot be rehabilitated. When windows must be replaced, they should be of the same style, size and material as the historic window. Though some houses in the neighborhood contain elaborate window treatments such as stained glass, ornate windows should not be added unless there is evidence of their existence historically. Replacement panes should be clear, not tinted. The number of panes should be the same as the historic window with the frame, mullions, and muntins of the same dimensions. *Et. fake muntins ("snap-ins") in replacement windows are not appropriate.*



Removing original windows robs a house of its historic character.

LCI Recommendations:

- Create village identities
- Promote mixed-use development
- Create balanced transportation solutions
- Design for pedestrian-friendly environments
- **Refocus and refine historic preservation efforts**



The appearance of original foundations should be maintained.

The foundations of many houses in the area are brick or stone piers. This characteristic is a common feature of historic houses in Georgia and should be retained. Traditionally the space between the piers was left open or filled with a simple covering. If the space is to be infilled, new material should be recessed four to six inches behind the original piers to create a shallow reveal. The fill should be masonry which matches the existing piers in mortar, color, shape, brick size and pattern. Infilling with concrete block is not appropriate. Where inappropriate infilling has occurred, the negative impact of the new material should be mitigated by applying a smooth stucco and painting this a dark color. Foundation repair should use materials that match the historic materials and use appropriate mortar.

Porches should be maintained and preserved.

Porches were historically both an important means of staying cool and of social interaction. Original porches and their elements such as columns, railings and brackets should not be removed. When rehabilitation of historic porches becomes necessary, every effort should be made to preserve viable materials. Replacement materials should closely match the original. Replacing original wooden columns and railings with wrought iron is inappropriate. Enclosure of porches in the historic district is strongly discouraged. If enclosure of a porch is allowed, the enclosing material should be a clear, transparent material in order to retain the "open" character of the porch. The enclosure should be recessed to allow the visual integrity of the historic elements to remain intact.



The original design and architectural elements of porches should be preserved and maintained.



Supports should not be replaced with wrought iron.



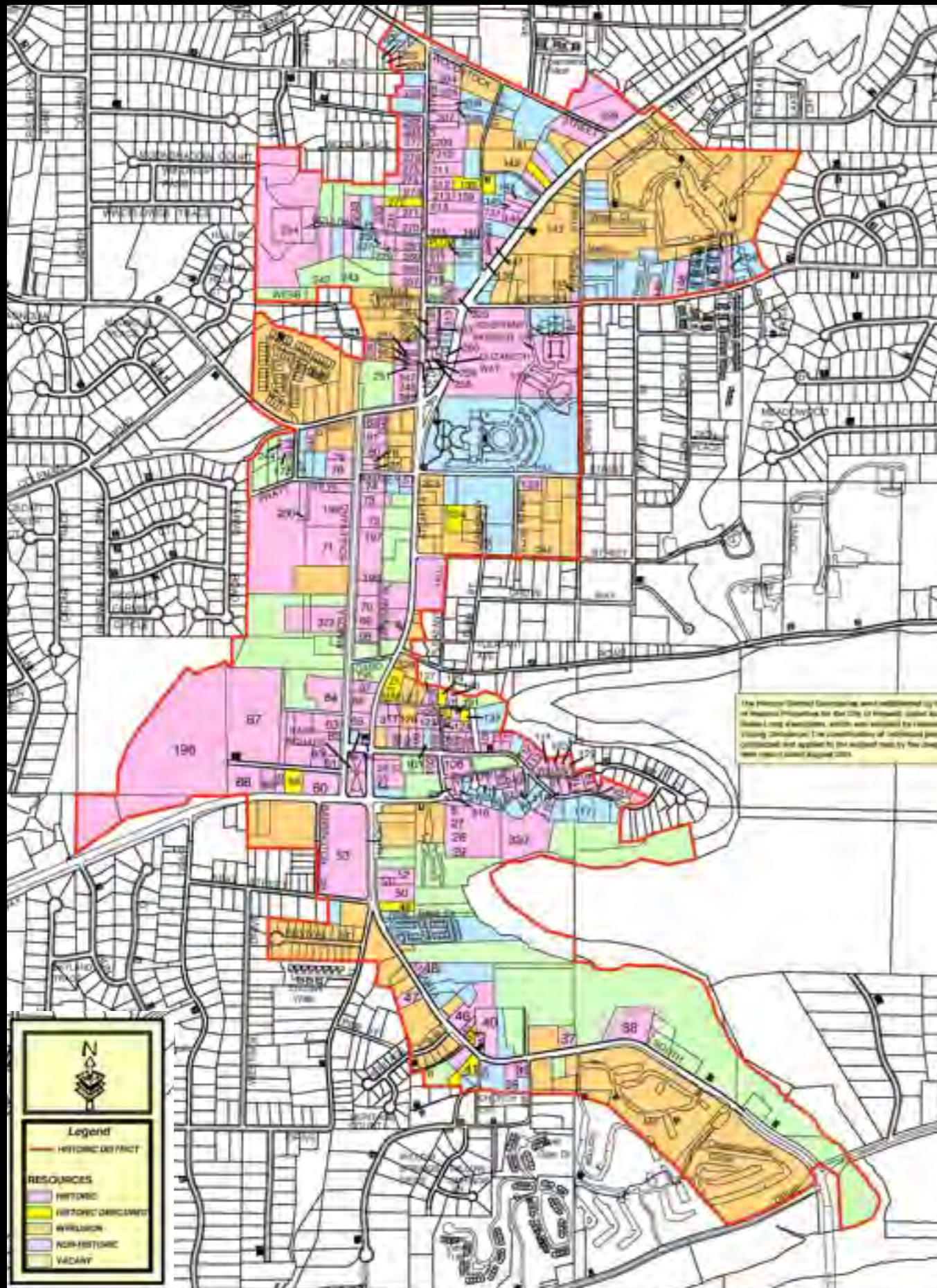
If a porch is enclosed, the architectural elements should not be covered and the material used should be transparent.



Porches should not be enclosed.

Update Recommendation:

Utilize Historic Design Guidelines as criteria in establishing preservation goals for specific buildings. Consider rating all Historic buildings in advance of any permit applications.

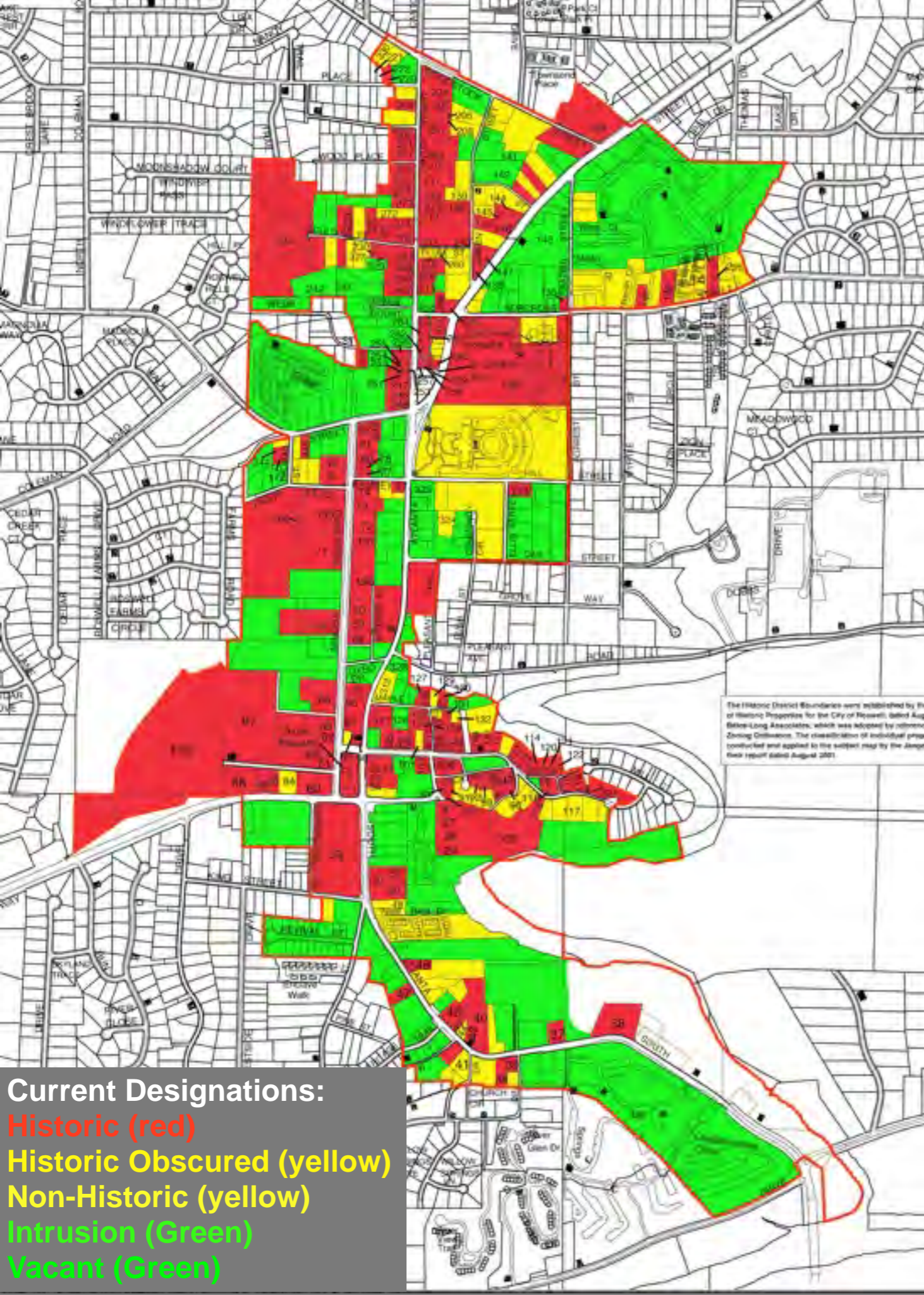


LCI Recommendations:

- Create village identities
- Promote mixed-use development
- Create balanced transportation solutions
- Design for pedestrian-friendly environments
- **Refocus and refine historic preservation efforts**

Update Recommendation:

Amend historic designation categories to be more clear, intentional, and decisive about historic preservation goals.



LCI Recommendations:

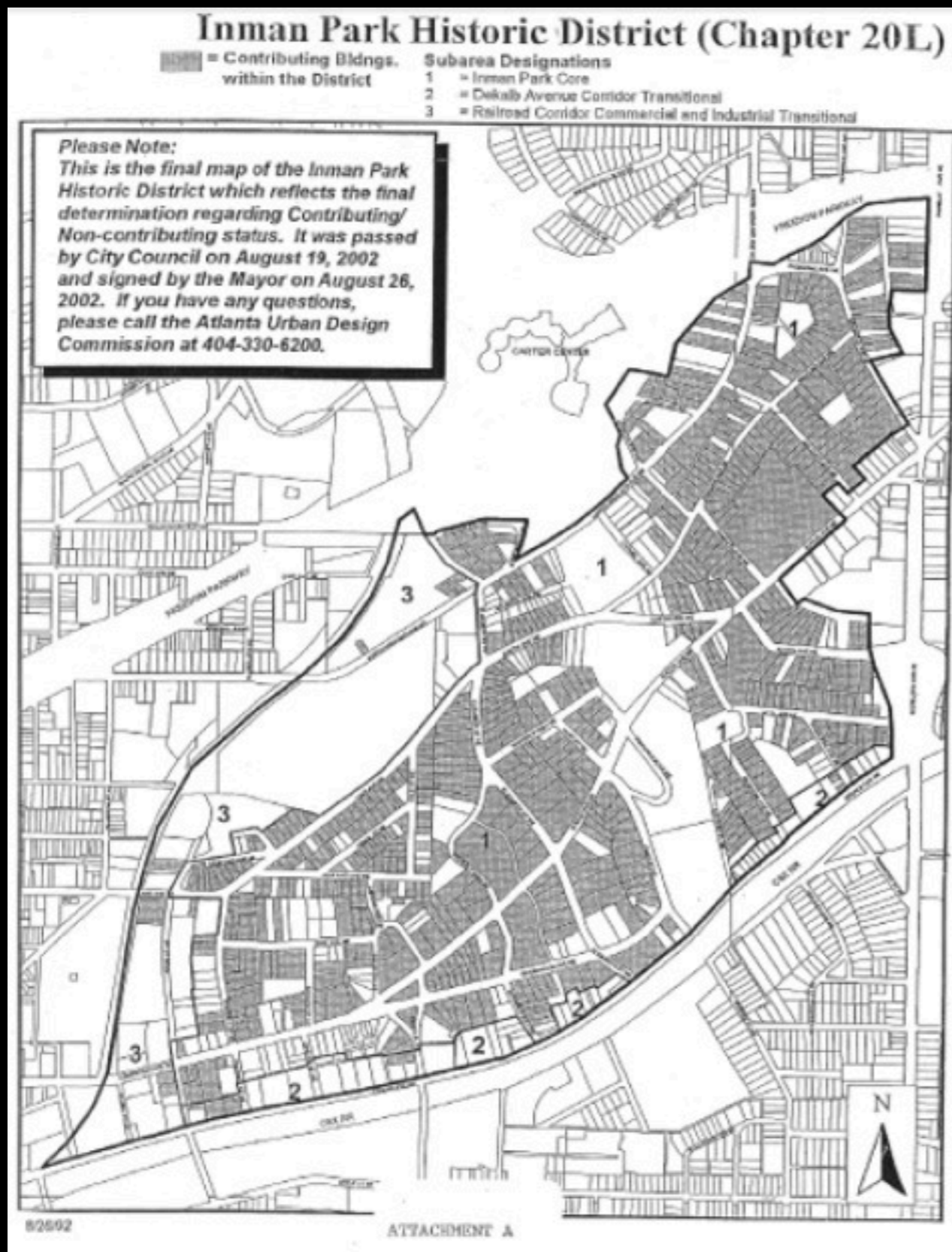
- Create village identities
- Promote mixed-use development
- Create balanced transportation solutions
- Design for pedestrian-friendly environments
- **Refocus and refine historic preservation efforts**

Update Recommendation:

Shift to contributing and non-contributing categories of historic designation

Current Designations:

- Historic (red)
- Historic Obscured (yellow)
- Non-Historic (yellow)
- Intrusion (Green)
- Vacant (Green)



LCI Recommendations:

- Create village identities
- Promote mixed-use development
- Create balanced transportation solutions
- Design for pedestrian-friendly environments
- **Refocus and refine historic preservation efforts**

Update Recommendation:

Consider place history as well the building history in determining preservation goals by establishing:

1. the place- contexts to be preserved, and
2. the buildings that contribute to the place context.

Individual buildings that are important to Roswell's history, but located in non-critical place-contexts can be protected differently than buildings that help define historically resonant places.

Inman Park in Atlanta is an example of a historic district structured on contributing/ non-contributing building designation categories.



H I S T O R I C
G A T E W A Y

ROSWELL, GEORGIA

DESIGN STANDARDS

DRAFT DOCUMENTS

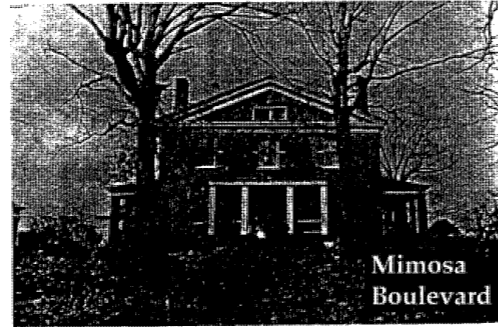
MAY 11

Deliverable
Document:

Design Standards

Design Standards

Roswell Historic District Design Guidelines



Adopted by Mayor and City Council on December 1, 1997

Roswell Historic District Design Guidelines



Mill Village and
South Atlanta Street

Roswell Historic District Design Guidelines



Adopted by Mayor and City Council on December 1, 1997

Update Recommendation:
Consolidate into a single document.
Address any deficiencies.

Design Standards

Background	
Introduction	.....
Procedures	.....
Maps	.....
Roswell's History	.....
Roswell's Significance	.....
Mill Village and South Atlanta Street Characteristics	.....
Architecture	
Styles	.....
Forms	.....
Commercial	.....
Residential Guidelines	
Site and Setting	.....
Trees	.....
Retaining walls	.....
Fencing	.....
Paving materials	.....
Driveways	.....
Parking	.....
Exterior lighting	.....
Signs	.....
Mechanical systems	.....
Outbuildings	.....
Street curbing	.....
Archeology	.....
Rehabilitation	.....
Roof shape and pitch	.....
Roofing materials	.....
Chimneys	.....
Doors	.....
Screen doors	.....
Storm windows	.....
Windows and details	.....
Window shutters	.....
Architectural detail	.....
Foundations	.....
Porches	.....
Exterior wood siding	.....
Access	.....
Fire escapes	.....
Residential Guidelines cont.	
New Construction and Additions	.....
Setbacks	.....
Spacing	.....
Orientation	.....
Height and width	.....
Roofs	.....
Exterior materials	.....
Windows and doors	.....
Foundation materials	.....
Foundation height	.....
Porches	.....
Architectural details	.....
Other Issues	.....
Maintenance	.....
Demolition	.....
Relocation of buildings	.....
Commercial Guidelines	
Site and Setting	.....
Street furniture	.....
Sidewalks	.....
Parking	.....
Mechanical systems	.....
Streetlights	.....

Commercial Guidelines cont.	
New Construction and Additions	.....
Setbacks	.....
Height and width	.....
Spacing and orientation	.....
Materials	.....
Storefront and upper facade	.....
Upper windows	.....
Other Issues	.....
Maintenance	.....
Demolition	.....
Relocation of buildings	.....
Appendix	
Secretary of the Interior's Standards for Rehabilitation	.....
Roswell's Historic Preservation Ordinance	.....
Glossary	.....

Update Recommendation:

Bring Forward Good Information in the Existing Document:

- History Summary
- Permit and Review Processes
- General guidance on critical elements of historic buildings
- General guidance on maintenance, preservations and compatible new construction
- Ordinances

Design Standards



Update Recommendation:

Integrate Design Guidelines with Zoning, and place an emphasis on compatibility rules such as:

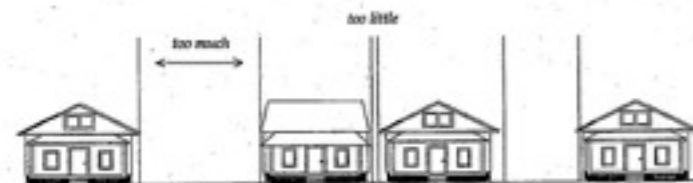
The element in question (roof form, architectural trim, etc.) shall compliment, match, or respond to that which predominates on the contributing buildings of the same block face or, where quantifiable (i.e., buildings height and width as measured at front facade, floor height, lot dimensions, etc.), no smaller than the smallest or larger than the largest such dimension of the contributing buildings of the same block face.

Spacing 35

The pattern of space between houses should not be altered by new construction. New buildings should continue the general pattern of spacing between buildings. Additions to existing buildings should not greatly reduce the existing space between the original structure and those adjacent. Additions should be constructed behind the plane of the original front facade to maintain the perception of the original spacing.



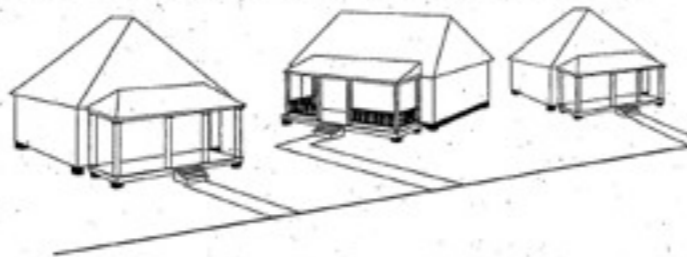
New construction should respect the pattern of spacing created by the historic houses.



Introducing different spacing breaks the characteristic rhythm created by the existing structures.

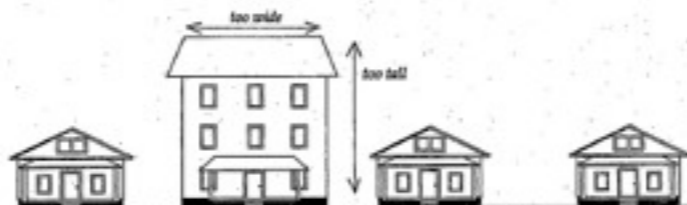
Orientation 30

Orientation of new construction to the road should follow the traditional pattern. The main entrance of the building should face the road, not the side or rear of the lot.



Height and width 31

New construction should be compatible in height and width with adjacent structures. Building heights range from one to two stories in the area. New construction should continue this pattern and it is best that it not vary more than ten percent (10%) in height with adjacent structures. New construction should maintain the traditional widths of the neighborhood. The height of additions to buildings should be consistent with the original structure.



New construction should be in scale with the existing structures.

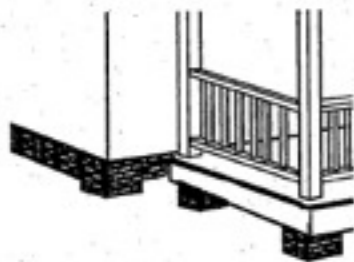
Design Standards

Update Recommendation:

Integrate with Zoning and place an emphasis on compatibility rules.

Foundation materials 35

The use of masonry foundations should be continued. The use of masonry and masonry piers for foundations is predominant in the district and should be continued. Concrete slab or concrete block foundations are not appropriate.



Foundation height 36

New foundation construction should match the height of adjacent structures. Historic structures or in the area are built on raised foundations. Foundation heights often increase on secondary and rear facades due to grade changes on the lot. New buildings should be constructed with foundations of at least one foot above grade and should be within ten percent (10%) of the average foundation height of adjacent structures. Foundation heights of additions should match the original structure. No building should be constructed at grade.

Porches 37

New buildings should continue the pattern of front porches. Front porches are a defining characteristic of the neighborhood. Porches on new buildings are strongly encouraged. They should be compatible in scale and materials with the house and with adjacent and surrounding buildings.

Architectural Details 38

Architectural details which give the building a "sense of belonging" to the neighborhood are appropriate. The architectural details of new construction should relate in scale, rhythm, proportion, and texture to adjacent and surrounding historic buildings.

Roofs 32

New construction should respect roof pitches, shapes, orientation, and materials found in the district.



The roofs of new buildings should relate in shape, pitch, orientation, and materials to the roofs of adjacent and surrounding buildings. Roof shapes of houses in the area are generally gabled, hipped, or pyramidal. Roofs on additions should match that of the original building in shape, pitch, and materials. Roof decks, dormers, skylights, solar panels, and communications equipment should be carefully placed to be compatible and to have the least negative impact on the exterior appearance of the site, structure, and surrounding buildings.

Exterior Materials 33

New buildings should use exterior materials common to buildings in the district. Wooden clapboard and brick is overwhelmingly used in the district and is encouraged on new construction. Additions should use materials matching those of the original building. Synthetic siding of appropriate scale and smooth texture (no simulated wood grain) may be acceptable on new buildings. Stucco, dry-vit, concrete, and metal are not appropriate exterior materials.

Windows and Doors 34

New construction should respect the pattern of windows and doors of buildings in the vicinity.

Windows and doors should be compatible in scale, size, and proportion to established patterns of those in surrounding buildings. The ratio of openings to solids (e.g., windows and doors to walls) shall be similar to those of surrounding buildings. Windows in additions should carefully match those in the original portion of the building. An addition should not introduce a new main entrance on the front facade.

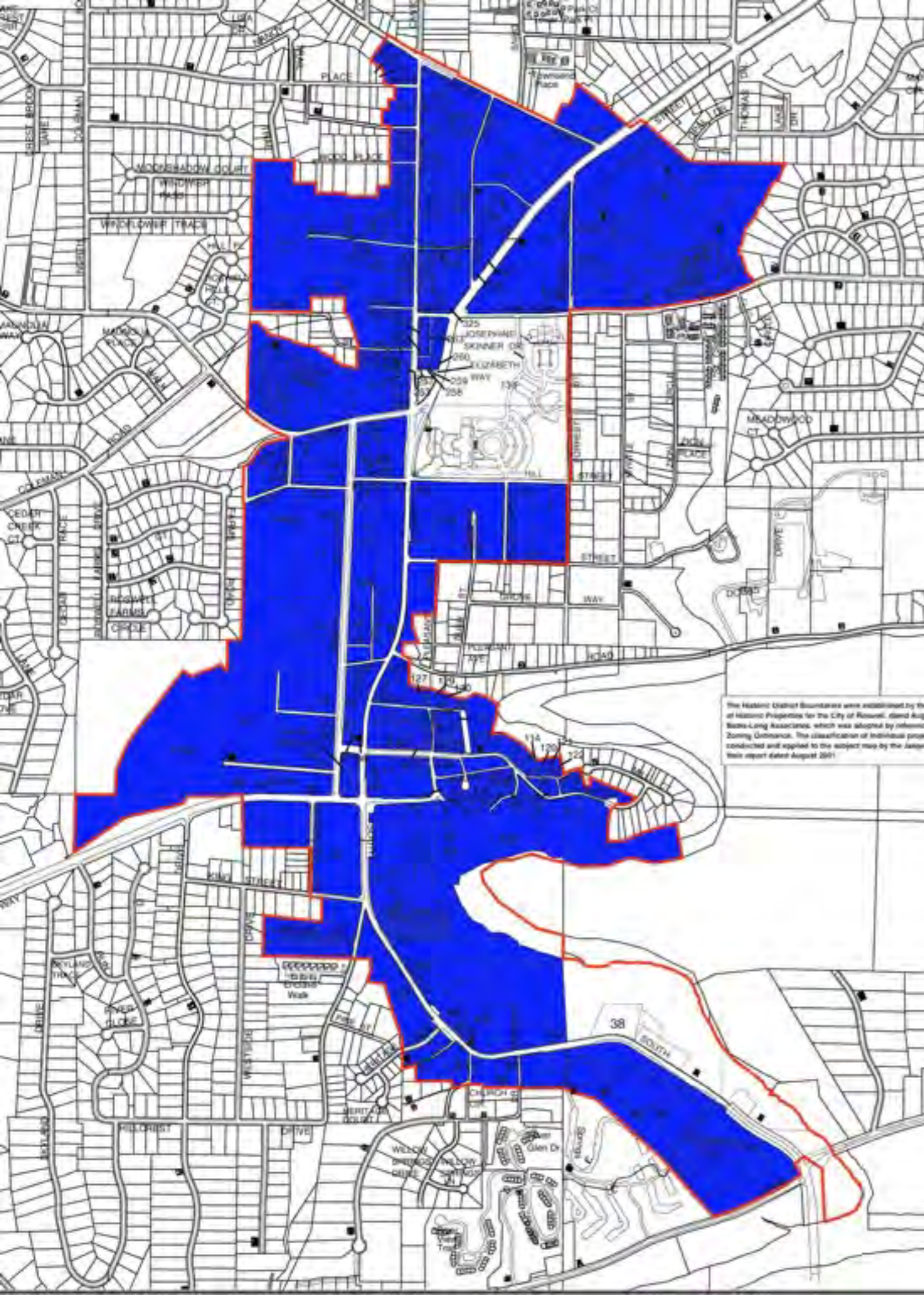


YES!

NO!

Design Standards

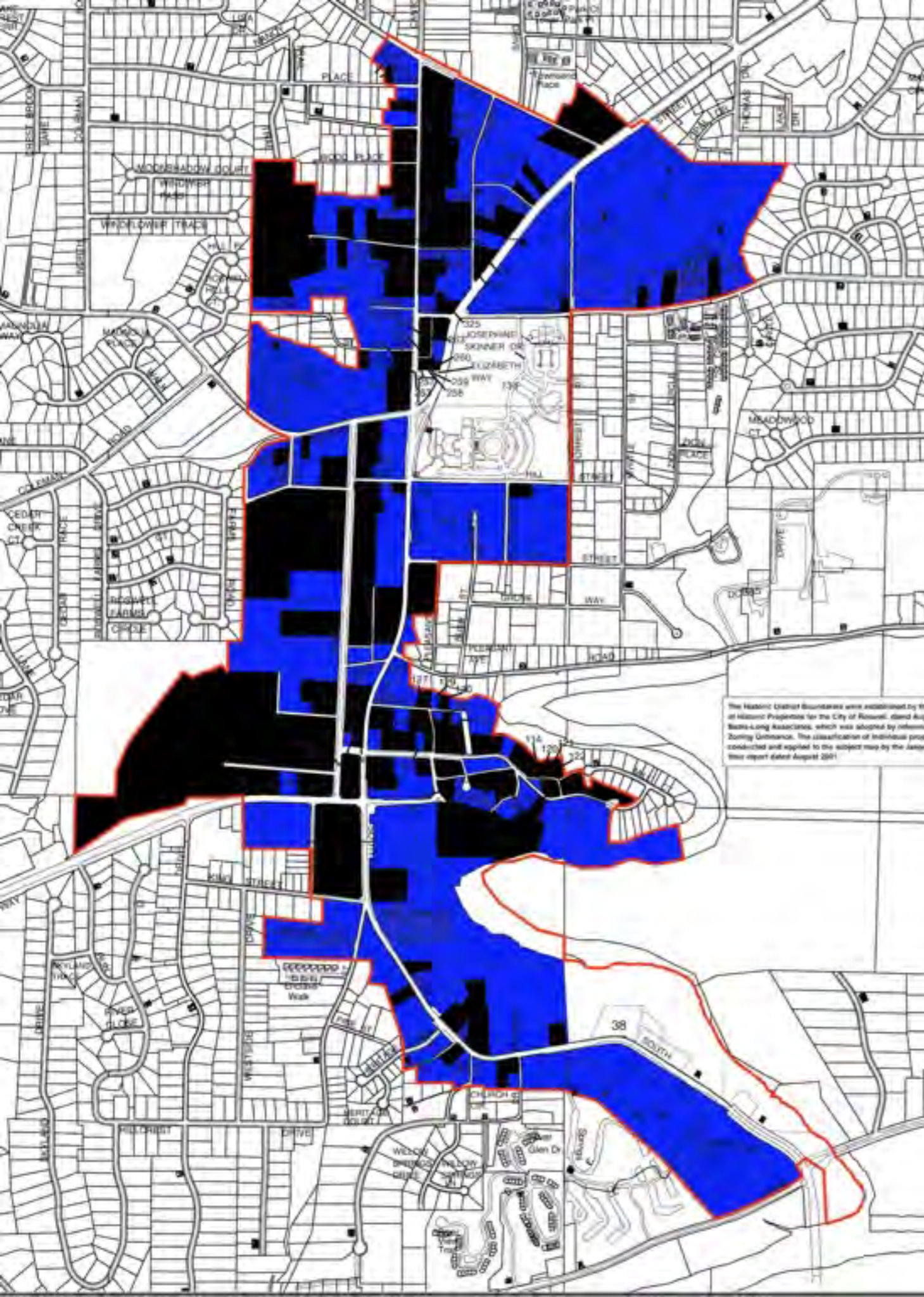
Areas to apply land use regulations shown in blue.



Design Standards

Areas to apply land use regulations shown in blue.

With parcels blacked out that are best regulated by historic Preservation regulations.

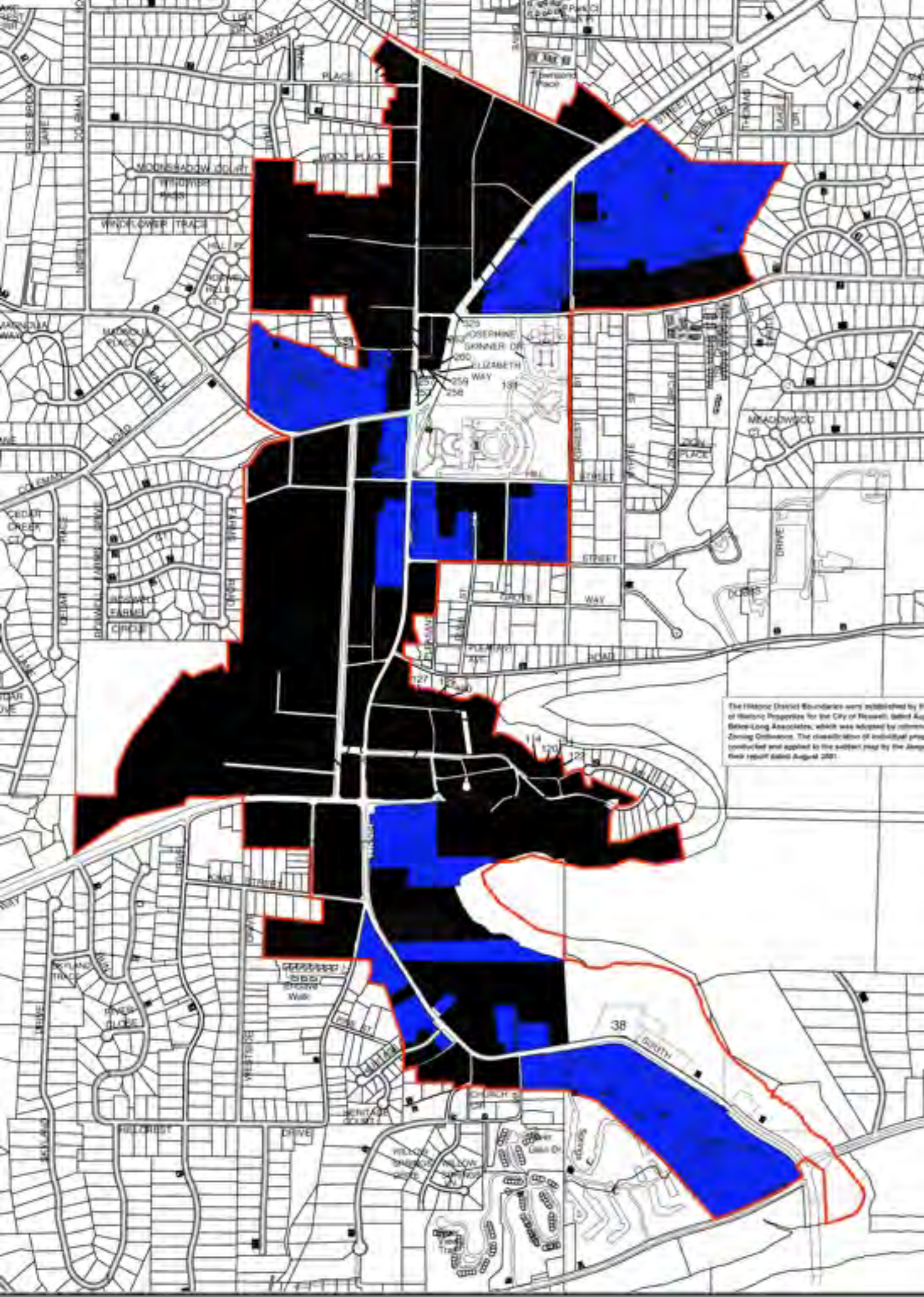


Design Standards

Areas to apply land use regulations shown in blue.

With parcels blacked out that are best regulated by historic Preservation regulations.

And with parcels that have sufficient context to exercise compatibility provisions as land use regulations



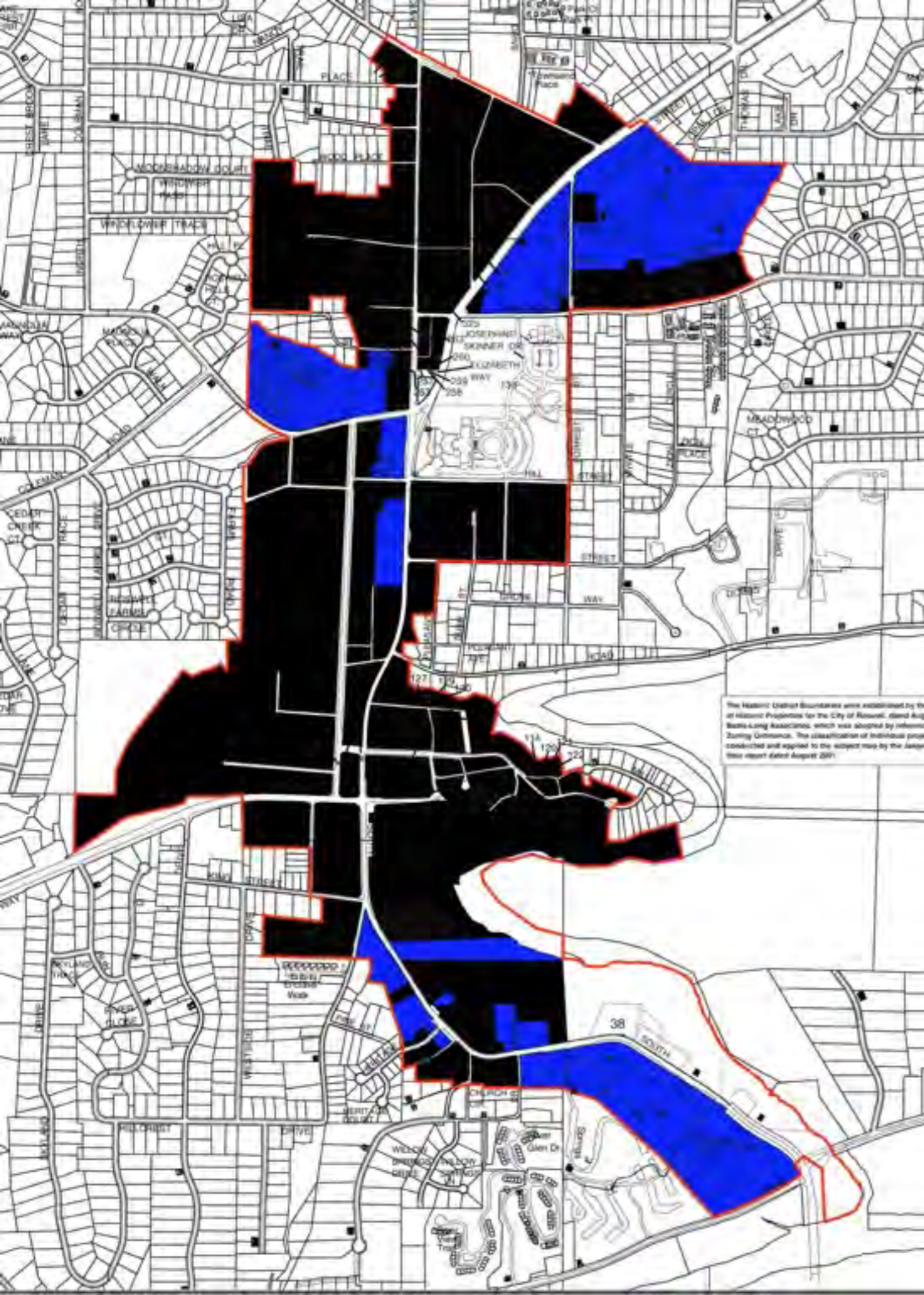
Design Standards

Areas to apply land use regulations shown in blue.

With parcels blacked out that are best regulated by historic Preservation regulations.

And with parcels blacked out that have sufficient context to exercise compatibility provisions as land use regulations

And with areas blacked out that have a specific code already in place.



Design Standards

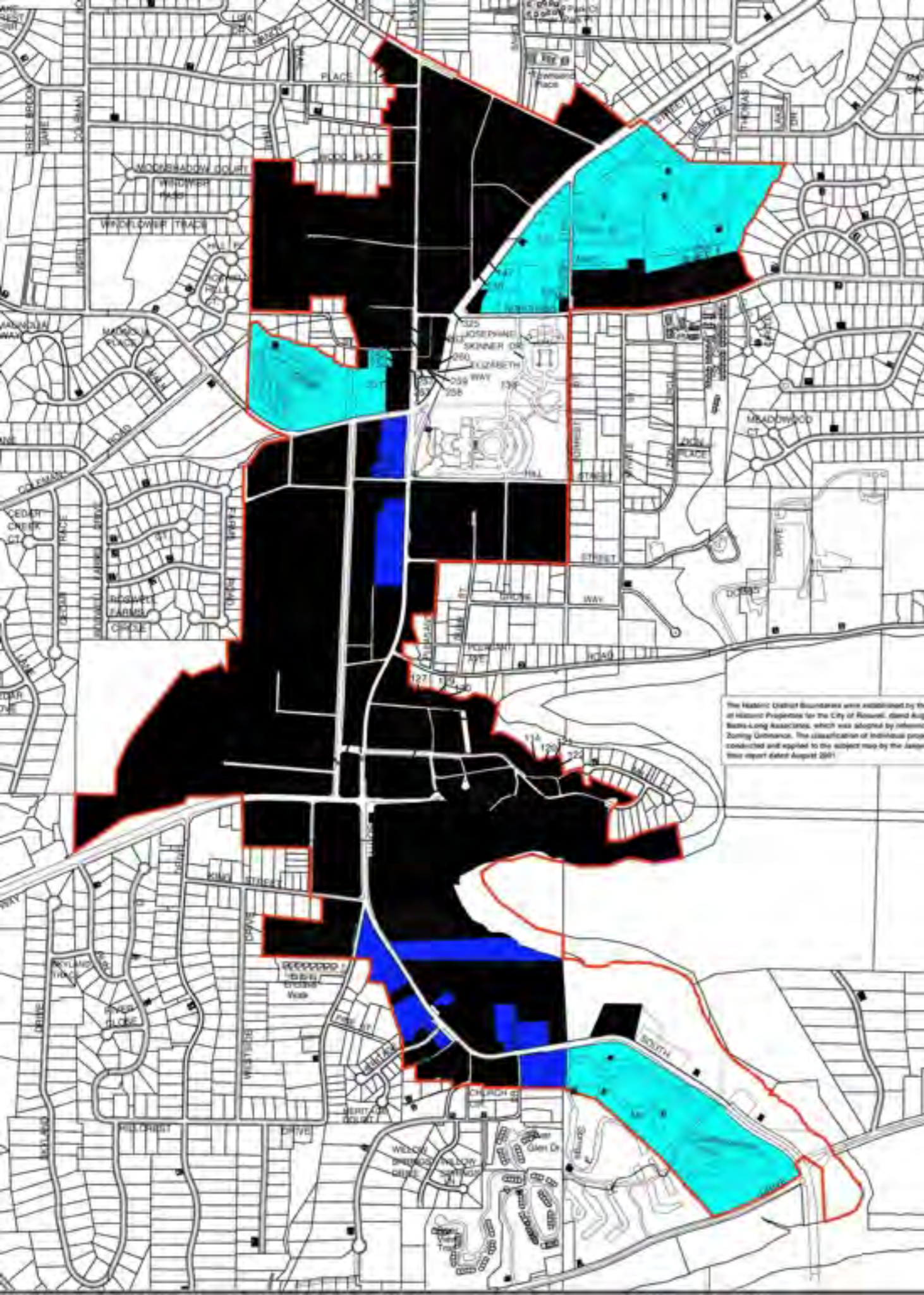
Areas to apply land use regulations shown in blue.

With parcels blacked out that are best regulated by historic Preservation regulations.

And with parcels blacked out that have sufficient context to exercise compatibility provisions as land use regulations

And with areas blacked out that have a specific code already in place.

And showing areas that should be considered Special Districts in teal.



Design Standards

Areas to apply land use regulations.

With parcels blacked out that are best regulated by historic Preservation regulations.

And with parcels blacked out that have sufficient context to exercise compatibility provisions.

And with areas blacked out that have a specific code already in place.

And showing areas that should be considered Special Districts in teal.

And showing parcels that are addressed by the scroll in Purple.

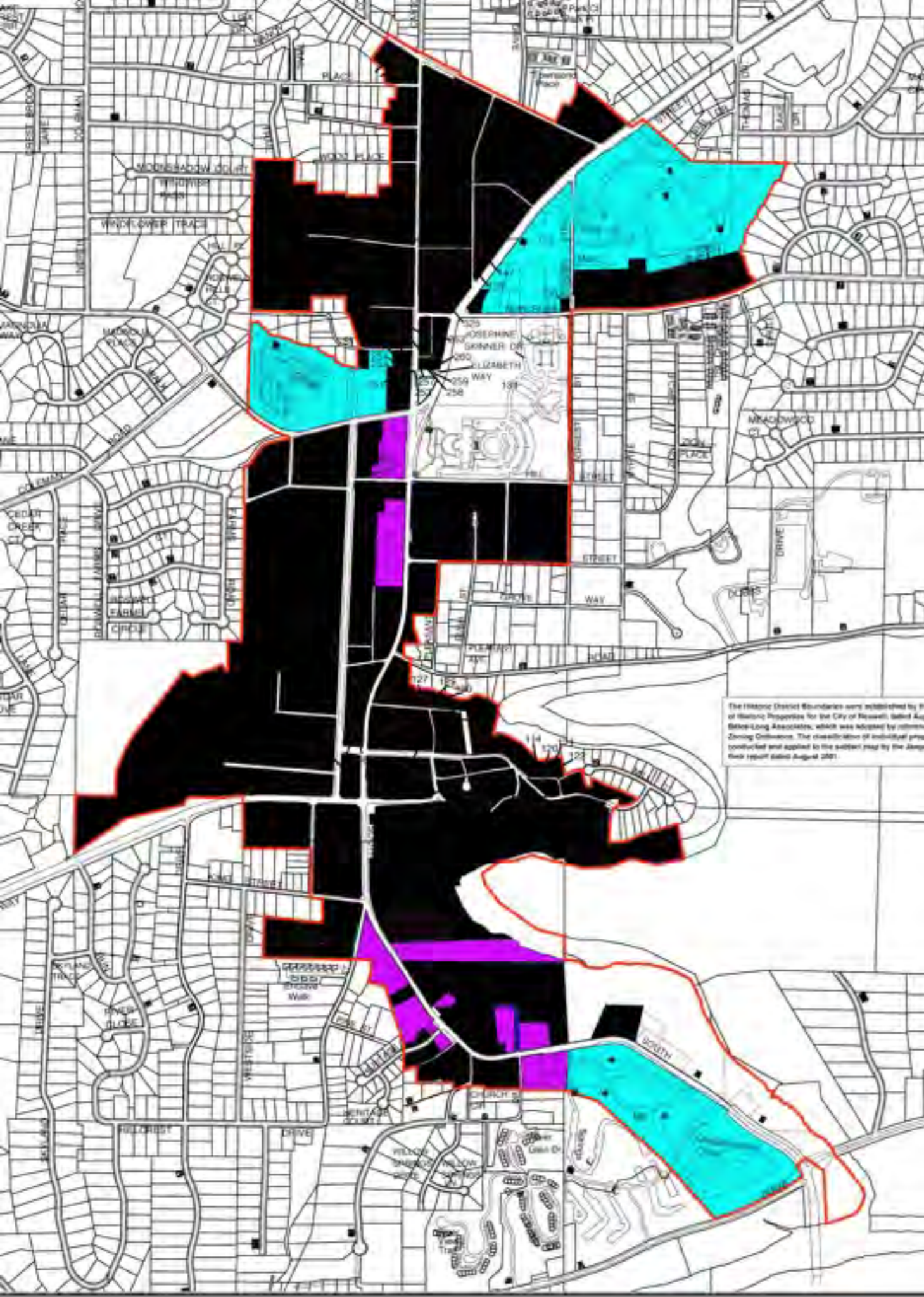
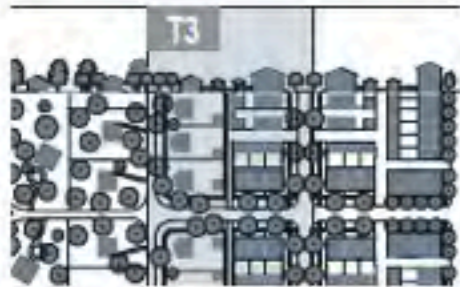


TABLE 5B. T3 BUILDING DISPOSITION AND CONFIGURATION



BUILDING FUNCTION	
Residential	restricted use
Lodging	restricted use
Office	restricted use
Retail	restricted use

BUILDING CONFIGURATION	
Principal Building	2 stories max.
Outbuilding	2 stories max.
Structured Parking	not applicable

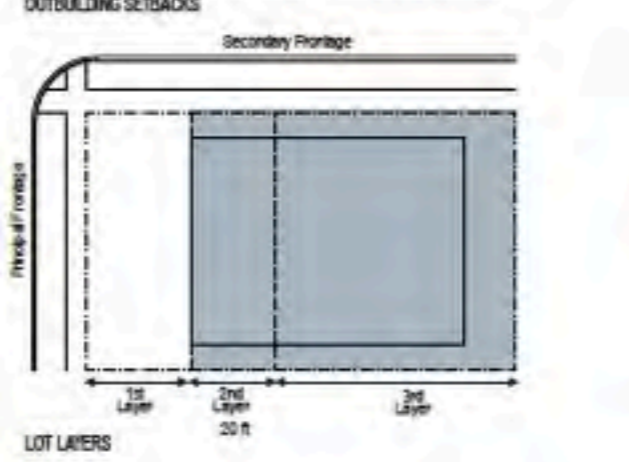
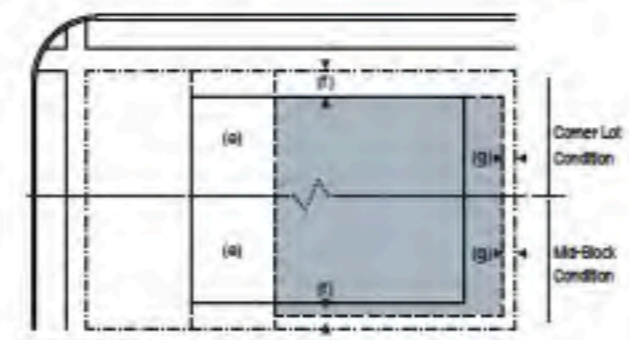
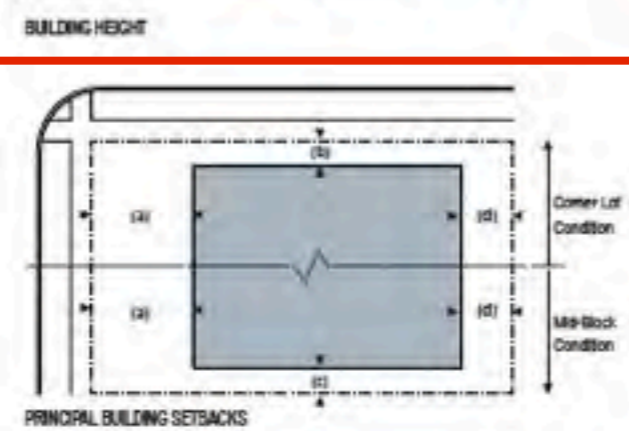
LOT OCCUPATION	
Lot Width	54 ft. min. 250 ft. max.
Lot Coverage	60% max.

BUILDING DISPOSITION	
Edgeward	permitted
Sideward	not permitted
Keeward	not permitted
Courtyard	not permitted

SETBACKS - PRINCIPAL BUILDING	
(a) Front Setback Principal	15ft*
(b) Front Setback Secondary	12 ft. min.
(c) Side Setback	12 ft. min.
(d) Rear Setback	12 ft. min.**
Frontage Buildout	40% min. at setback

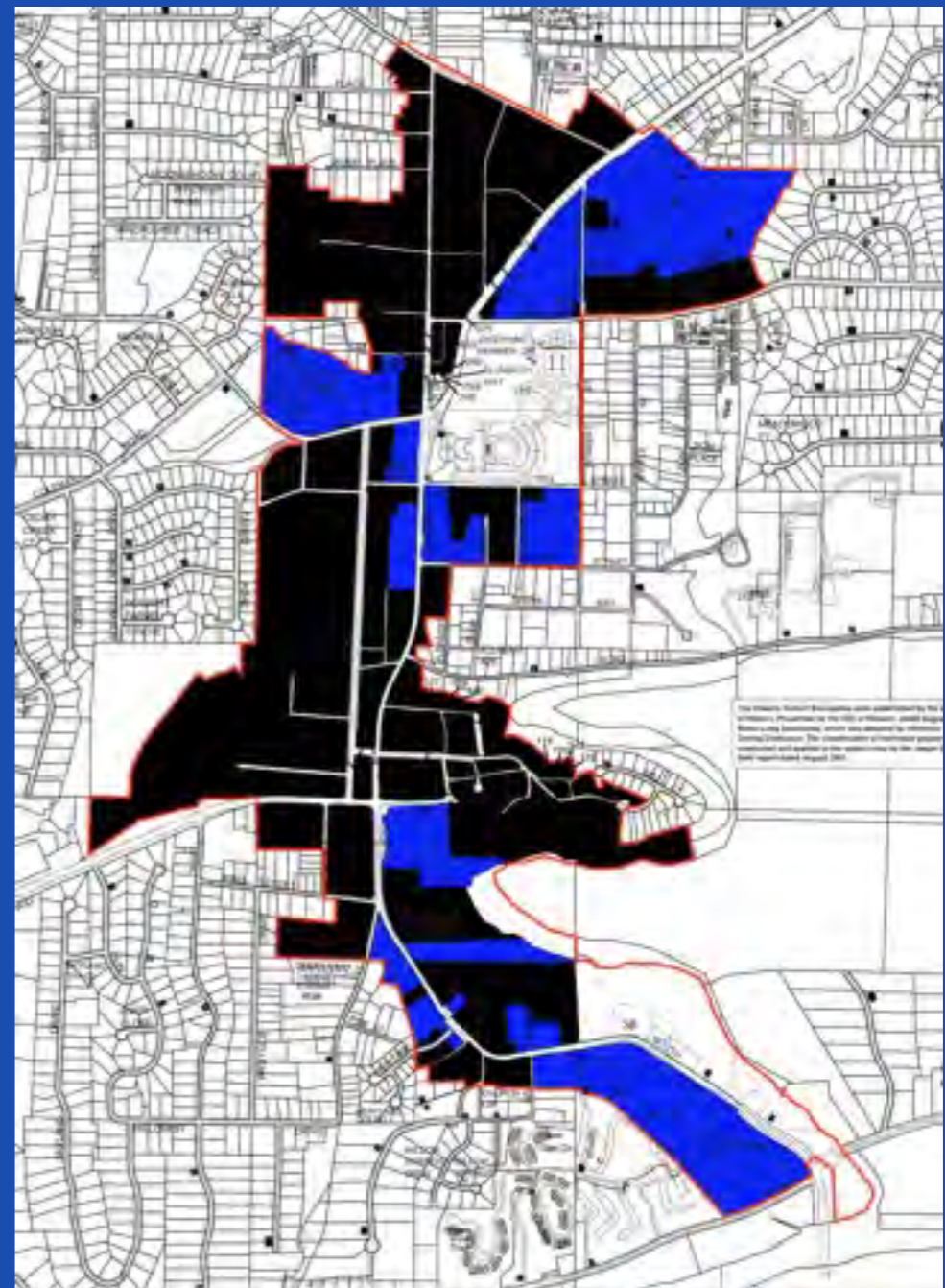
SETBACKS - OUTBUILDING	
(e) Front Setback	20 ft. min. + bldg. setback
(f) Side Setback	3 ft. min. or 6 ft. at corner
(g) Rear Setback	3 ft. min.

* or average of existing setbacks of buildings on nearest two lots but not less than 5 ft. or more than 30 ft.
 **or 15 ft. from center line of alley
 N stands for any stories above those shown, up to the maximum.
 Refer to metrics for exact minimums and maximums



Design Standards

Building Disposition Summaries



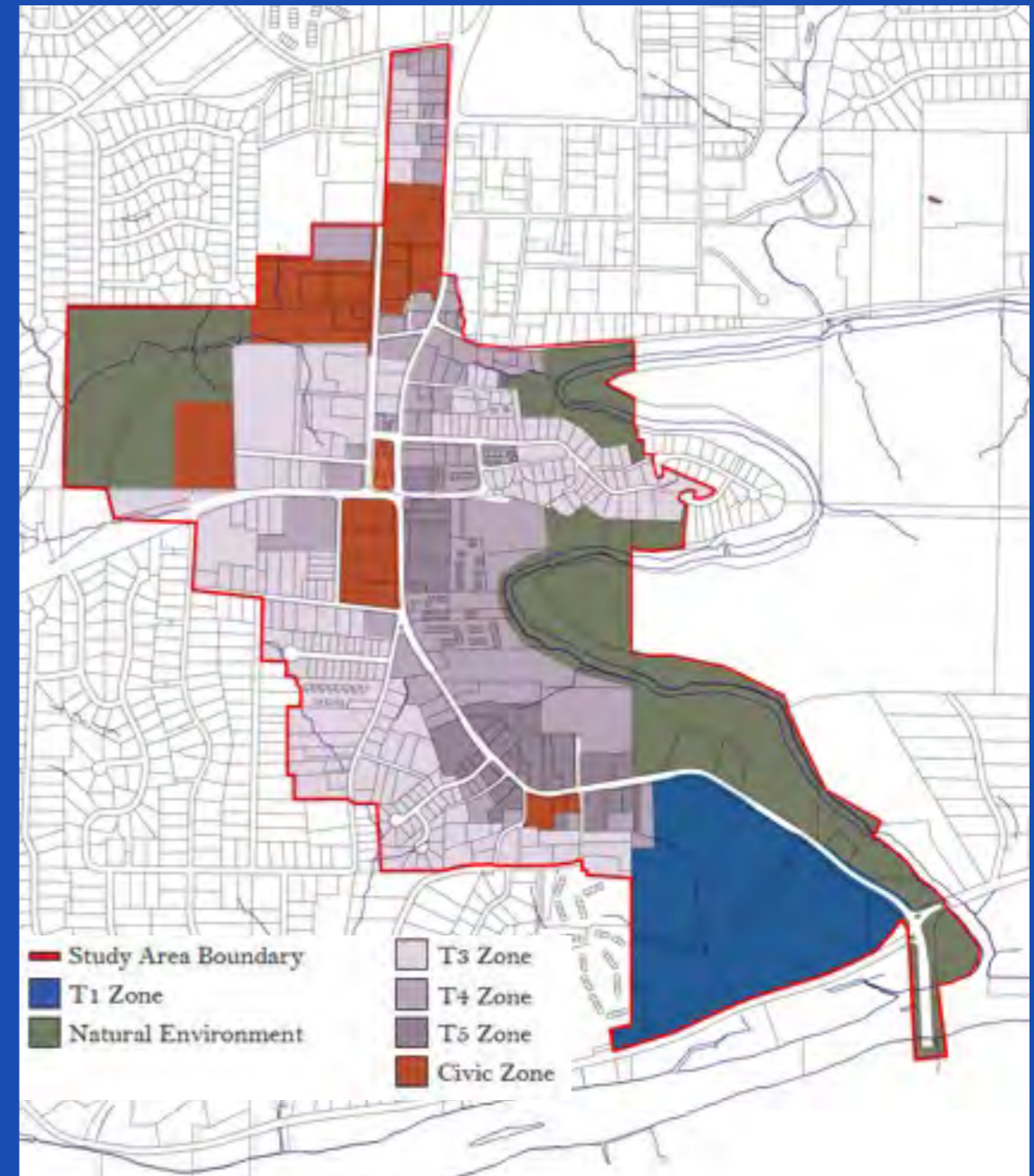
Regulations not needed in a Preservation Priority Areas if Compatibility Rules are applied.

Preservation Priority Areas shown in Black

Design Standards

Building Function Table should be applied to the whole district

TABLE 5D. BUILDING FUNCTION			
	T3	T4	T5
a. RESIDENTIAL	Restricted Residential: The number of dwellings on each lot is restricted to one within a principal building and one within an accessory building, with 1.5 parking places for each. Both dwellings shall be under single ownership.	Limited Residential: The number of dwellings on each lot is limited by the requirement of 1.5 parking places for each dwelling.	Limited Residential: The number of dwellings on each lot is limited by the requirement of 1.5 parking places for each dwelling.
b. LODGING	Restricted Lodging: The number of bedrooms available on each lot for lodging is limited by the requirement of 1.0 assigned parking place for each bedroom, up to five, in addition to the parking requirement for the dwelling. The lodging must be owner occupied. Food service may be provided in the morning. The maximum length of stay shall not exceed ten days.	Restricted Lodging: The number of bedrooms available on each lot for lodging is limited by the requirement of 1.0 assigned parking place for each bedroom, up to five, in addition to the parking requirement for the dwelling. The lodging must be owner occupied. Food service may be provided in the morning. The maximum length of stay shall not exceed ten days.	Limited Lodging: The number of bedrooms available on each lot for lodging is limited by the requirement of 1.0 parking place for each bedroom, up to twelve, in addition to the parking requirement for the dwelling. The lodging must be owner occupied. Food service may be provided in the morning. The maximum length of stay shall not exceed fourteen days.
c. OFFICE	Restricted Office: The building area available for office use on each lot is restricted to the first story of the principal or the accessory building or in basements. Parking requirements and maximum number of employees and occupants shall be determined by warrant.	Limited Office: The building area available for office use on each lot is limited to the first story of the principal building and accessory building, and by the requirement of 3.0 assigned parking places per 1,000 square feet of net office space in addition to the parking requirement for each dwelling.	Open Office: The building area available for office use on each lot is limited by the requirement of 2.0 assigned parking places per 1,000 square feet of net office space.
d. RETAIL	N/A	Limited Retail: The building area available for retail use is limited to the first story of buildings at corner locations and by the requirement of 3.0 assigned parking places per 1,000 square feet of net retail space in addition to the parking requirement of each dwelling. Food service shall be further limited to seating no more than 40 patrons.	Limited Retail: The building area available for retail use is limited by the requirement of 2.0 assigned parking places per 1,000 square feet of net retail space. Retail spaces under 1,500 square feet are exempt from parking requirements.
e. CIVIC	See Table 5F	See Table 5F	See Table 5F



Transect based Use tables can be applied throughout the Historic District

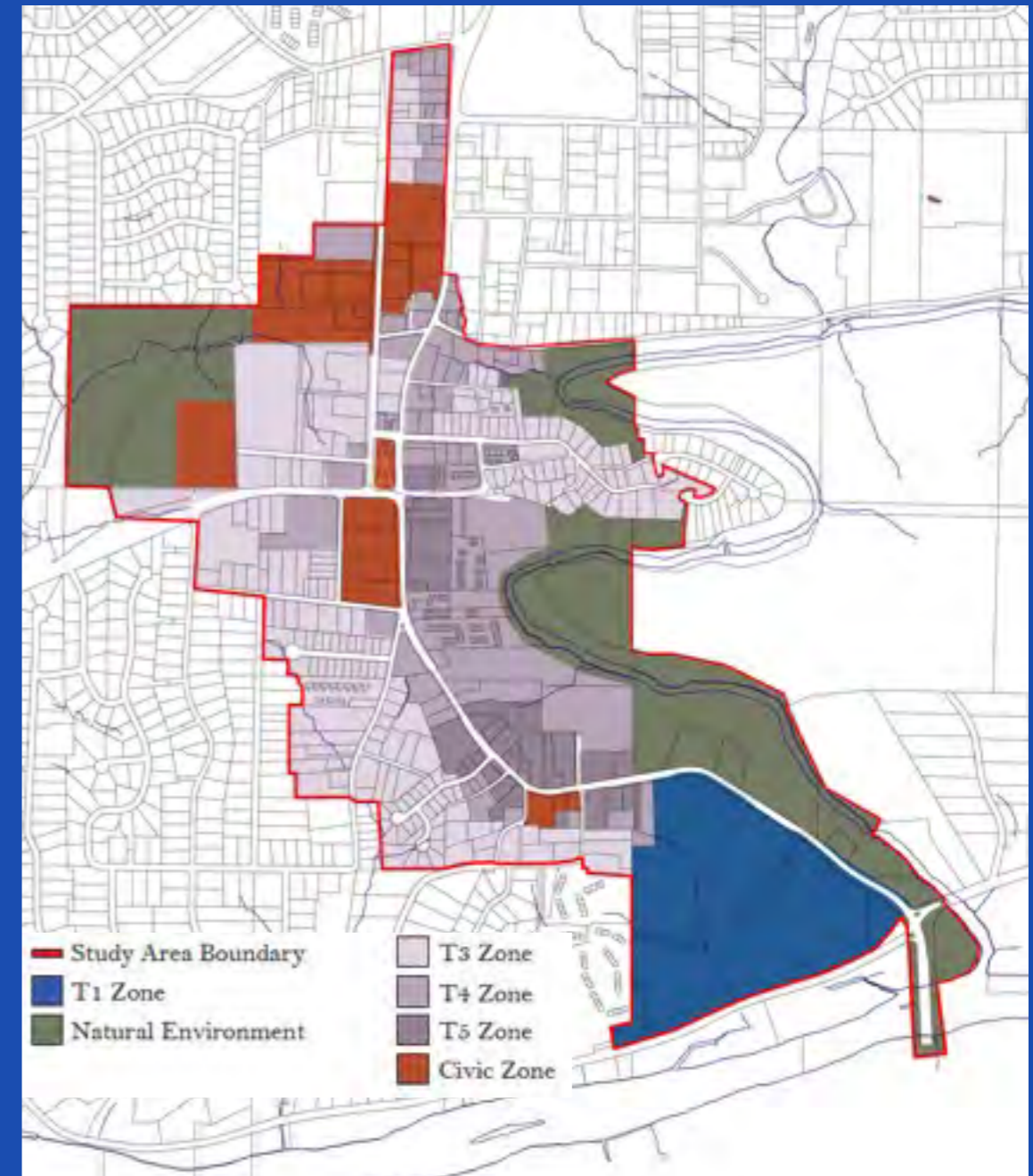
Transects are delineated by the Regulating Plan
This plan should be extended to the entire Historic District

Design Standards

Parking Requirements Table should be applied to the whole district

TABLE 5F. PARKING REQUIREMENTS AND STANDARDS							
1. OTHER	See Table 5F	See Table 5F	See Table 5F				
REQUIRED PARKING							
	T3	T4	T5				
RESIDENTIAL *	2.0 / dwelling	1.5 / dwelling	1.0 / dwelling				
LODGING	1.0 / bedroom	1.0 / bedroom	1.0 / bedroom				
OFFICE	3.0 / 1,000 sq. ft.	3.0 / 1,000 sq. ft.	2.0 / 1,000 sq. ft.				
RETAIL	3.0 / 1,000 sq. ft.	3.0 / 1,000 sq. ft.	2.0 / 1,000 sq. ft.				
CIVIC	BY WARRENT						
OTHER	BY WARRENT						
SHARED PARKING TABLE							
	PEAK HOUR	M-F	M-F	M-F	Sat & Sun	Sat & Sun	Sat & Sun
USE	REQUIRED PARKING BY USE	8am - 6pm	6pm - 12am	12am - 8am	8am - 6pm	6pm - 12am	12am - 8am
RESIDENTIAL	n =	60%	100%	100%	80%	100%	100%
LODGING	n =	70%	100%	100%	70%	100%	100%
OFFICE	n =	100%	20%	5%	5%	5%	5%
RETAIL	n =	90%	80%	5%	100%	70%	5%
CIVIC (NON-CHURCH)	n =	100%	20%	5%	10%	10%	5%
CIVIC (CHURCH)	n =	20%	20%	5%	100%	50%	5%
SHARED PARKING REQUIREMENT	SUM =	SUM =	SUM =	SUM =	SUM =	SUM =	SUM =
ADJUSTED PARKING REQUIREMENT	HIGHEST SHARED PARKING REQUIREMENT USAGE =						

Note: n = Required parking by use as determined by the required parking table



Transect based parking tables can be applied throughout the Historic District

Transects are delineated by the Regulating Plan
This plan should be extended to the entire Historic District

Design Standards

DESIGN STANDARDS | URBAN GUIDELINES DETAILED DISCUSSION FOR ALL TRANSECTS

5. URBAN STANDARDS

5.1 INTENT

- 5.1.1 Urban Standards regulate the development and modification of buildings and other elements of the built environment within the private lot, based on the following premises:
- That building regulations should equitably balance the rights of the individual and the interests of the community as a whole.
 - That building form individually and collectively defines and supports the public realm.
 - That building configuration should support walkability, safe streets, and safe public spaces, creating pedestrian friendly neighborhoods.
 - That building scale should define streets and public spaces as rooms, and should vary by context and intensity in coordination with neighboring properties.

5.2 INSTRUCTIONS

- 5.2.1 This Article sets forth the standards applicable to the development and modification of buildings and other elements of the built environment within private lots, by transect zone, in the community units to which it applies.
- 5.2.2 Plans required by this Section are subject to administrative approval by the RC.
- 5.2.3 Building and site plans submitted for approval shall demonstrate compliance with:
- Lot standards (Section 5.3)
 - Building disposition (Section 5.4)
 - Building configuration (Section 5.5)
 - Frontage standards (Section 5.6)
 - Building function (Sections 5.7)
 - Parking standards (Section 5.8)
 - Landscape standards (Section 5.9)
 - Nonconformities, if any (Section 5.10)
 - Special requirements, if any (Section 5.11)
 - Building materials and configuration (Section 5.12)

5.3 T-3 LOT STANDARDS

- 5.3.1 For the purposes of this Section, each lot shall be composed of three layers according to Table 5B.
- 5.3.2 The portion of a lot fronting a thoroughfare or a passage shall be designated its principal frontage. Corner lots shall have designated a principal frontage along the thoroughfare or passage of higher pedestrian importance and a secondary frontage along the remaining frontage. Lots, other than corner lots, fronting more than one thoroughfare or passage shall have their frontages determined by warrant and may be subject to more than one principal frontage.
- 5.3.3 Any portion of a lot mapped with a mandatory or recommended infill thoroughfare shall consider the new right-of-way as an access easement. Right-of-ways of mandatory or recommended infill thoroughfares may qualify as frontages. The planning office may determine a discretionary thoroughfare designation as an easement and frontage.
- 5.3.4 Newly platted lots, subdivisions of existing lots and newly assembled lots shall not exceed the following lot widths as measured along their principal frontage:

Update Recommendation:

Distinguish between Guidelines (advisory) and Codes (mandatory).

Generally Codes are established with text and illustrated by images.

Guidelines are generally established with images and supplemented with text.



HISTORIC
GATEWAY

ROSWELL, GEORGIA

CULTURAL RESOURCES

2ND DRAFT REPORT

MAY 2012

Deliverable
Document:
Cultural Resources
Report

Cultural Resources



Update Recommendation:

Review parcel update recommendations with Historic Preservation Commission

Convert to a Contributing/Non-Contributing property designation system.

Define the place-context of Historic Zones

Designate Redevelopment Priority Areas

Cultural Resources



Update Recommendation:

Several Mid-Century buildings, currently designated as Intrusions are now 50 years old and thus their historic status can be reconsidered. The auto-orientation of these buildings, with parking to the front and side, detracts from the pedestrian experience. Because of negative impacts to the pedestrian realm these buildings should continue to be considered intrusions.



HISTORIC
GATEWAY

ROSWELL, GEORGIA

MASTER PLAN

DRAFT REPORT
MAY 2012

Deliverable
Document:

Master Plan
Report

Master Plan

Existing Conditions



Master Plan



	070	080	090	070	080	090	080	090	070	080
PRIVATE LOT LAYER										
 zoning DISTRICT										
RESIDENTIAL										
COMMERCIAL										
INDUSTRIAL										
OFFICE										
RECREATION										
ARTS AND CULTURE										
UNEMPLOYED										
 FUNCTION										
RESIDENTIAL										
OFFICE										
RETAIL										
RESTAURANT										
RECREATION										
ARTS AND CULTURE										
UNEMPLOYED										
 PRIVATE FRONTAGE TYPE										
PROFESSIONAL										
RETAIL										
OFFICE										
RECREATION										
ARTS AND CULTURE										
UNEMPLOYED										
 SETBACK										
RETAIL										
OFFICE										
RECREATION										
ARTS AND CULTURE										
UNEMPLOYED										
 PUBLIC FRONTAGE LAYER										
 CURBING TYPE										
CONCRETE										
ASPHALT										
PAVED										
 BALKWAY TYPE										
CONCRETE										
ASPHALT										
PAVED										
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 PARKING LANE										
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 VEHICLE FLOW										
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CONCRETE										
ASPHALT										
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 MEDIAN TYPE										
CONCRETE										
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ASPHALT										
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 BI-CYCLE PROVISION										
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 TRANSIT PROVISION										
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Update Recommendation:

Integrate the Context Sensitive Design Score into the redevelopment design process for Atlanta Street.

As designs for sections of Atlanta Street finalize, integrate the Context Sensitive Design Score into land use ordinances.

Master Plan

Update Recommendation:

Develop LCI redevelopment recommendations into specific redevelopment plans (new construction shown in dark grey)



Master Plan

Update Recommendation:

Define three village centers and intensify redevelopment in those areas.



New Canton Street



Existing Conditions

Master Plan | Town Center Square

Update Recommendation:

Conceptualize the Town Center Square as an ornamental green and place for important events.

Restore the area's role as a town center by developing a new, pedestrian-oriented back street offset from the square. The back street is more capable of serving as both a regional draw and neighborhood main street than the square, which is heavily impacted by the volume of traffic converging at the Marietta Highway/Atlanta Street intersection.





Existing Conditions

Master Plan | Town Center Square

Proposal with hardware store intact



Master Plan | Town Center Square

Proposal with a grocery store replacing the hardware store





Rendering of pedestrian section of backstreet.

Civic Square



Existing Conditions





Existing Conditions



Southern Neighborhood Center



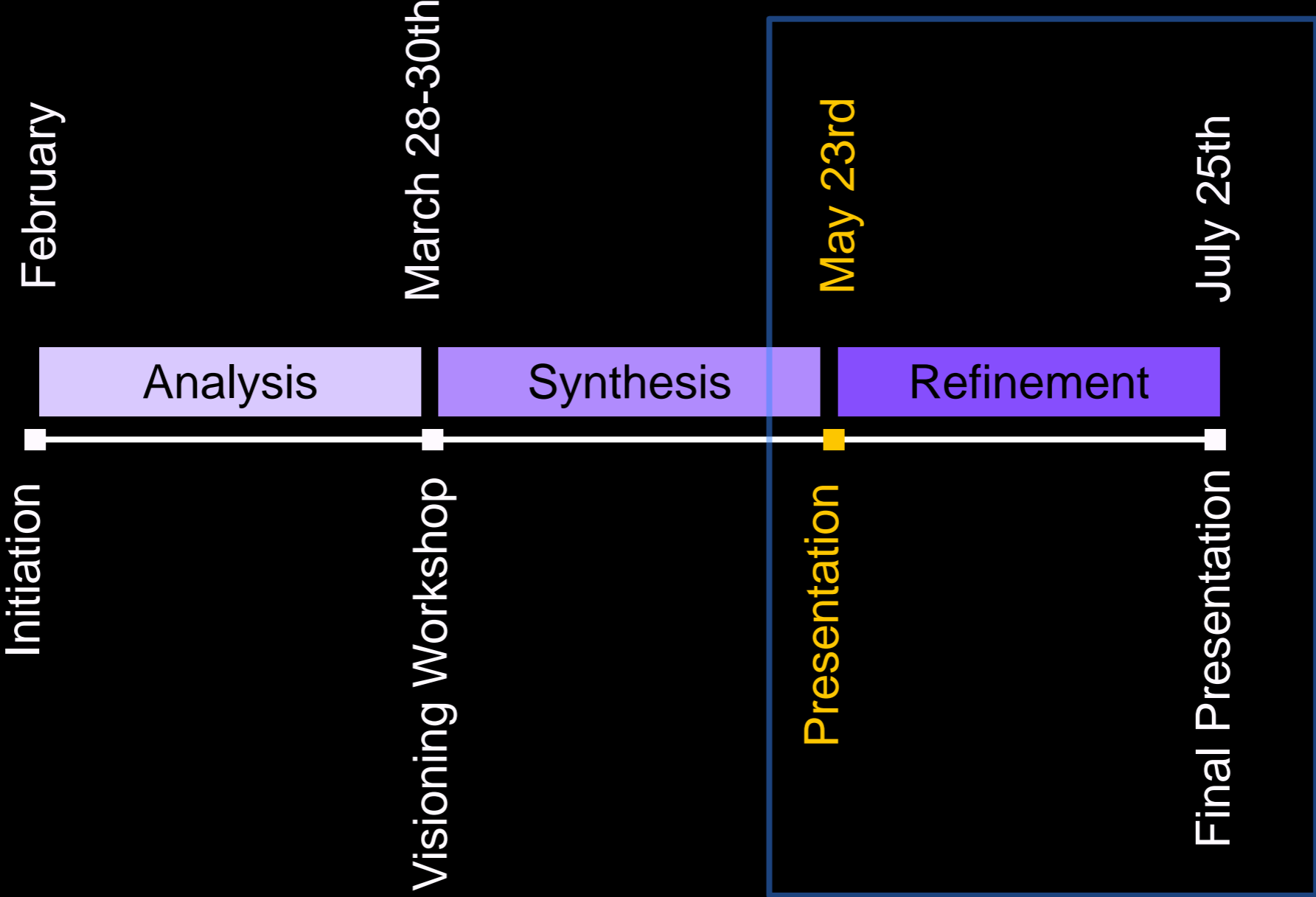
Existing Conditions

Existing Conditions





PLANNING PROCESS





HISTORIC
GATEWAY

ROSWELL, GEORGIA

MASTER PLAN

DRAFT REPORT
MAY 2012

Completing the Planning Process

Update Recommendations:

Master Plan Should Contain:

Overview

Background (pull forward from LCI plan)

A single Gateway District boundary

Urban Score

Three Centers Plans

View sheds/ Vistas plan

Standards for formation of Special Districts

Adopt as an updated plan that supplants the original LCI document



HISTORIC
GATEWAY

ROSWELL, GEORGIA

DESIGN STANDARDS

DRAFT DOCUMENTS

MAY 11

Completing the Planning Process

Update Recommendations:

Should Contain:

- Updated Historic Properties Map
- Establishment of redevelopment priority areas
- Standards for review of Historic Buildings
- Shift to contributing/ non-contributing designations
- Consolidated Design Guidelines (from existing)
- Compatibility rule(s)
- All special areas ordinances

Should be adopted by Council and/
or HPC



H I S T O R I C
G A T E W A Y

ROSWELL, GEORGIA

C U L T U R A L R E S O U R C E S

2ND DRAFT REPORT

MAY 2012

Completing the Planning Process

Update Recommendations:

Should contain the survey of historic properties.

The report is an advisory document prepared for the City Council and Historic Planning Commission

Approve final document (not for adoption)



HISTORIC
GATEWAY

ROSWELL, GEORGIA

MASTER PLAN

DRAFT REPORT
MAY 2012



Draft Master Plan Presentation

Scott Ball | Senior Project Manager | Duany Plater-Zyberk & Company | May 23, 2012